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# PLANNING JUSTIFICATION REPORT

CROFT STREET CONDOMINIUMS  
MUNICIPALITY OF PORT HOPE

*JANUARY, 2025*





211 DUNDAS STREET EAST, SUITE 202,  
BELLEVILLE ONTARIO K8N 1E2

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# 1.0 INTRODUCTION

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This Planning Justification Report has been prepared by RFA Planning Consultant Inc. on behalf of Leblanc Enterprises [the Owner] to support amendments to the Municipality of Port Hope Official Plan and Zoning By-law 2020/10. The Official Plan Amendment (OPA) and Zoning By-law Amendment (ZBA) applications seek to facilitate the redevelopment of the property at 0 Croft Street (the “subject site” or the “site”).

The subject site is located on the south side of the Croft Street extension, east of Wellington Street, and between Deblaquire Street North and Rose Glen Road North. The site currently is vacant land characterized by a dense, mature tree canopy/ woodland area. The site has an irregular shape and approximate area of 18,580 m<sup>2</sup> or 4.6 acres. The site is designated Residential - Medium Density in Schedule ‘C1’ - Land Use Urban Detail of the Municipality of Port Hope Official Plan (2017) and is zoned Low Density Residential One - Type One Holding Symbol 1 ‘RES-1(H1)’ as per the Zoning By-law 2020/10.

The OPA and ZBA applications requests to permit a six-storey residential apartment building with a total of 108 condominium dwelling units with each tower containing 54 units (Tower A and Tower B). Landscaped amenity space is proposed at a rate of approximately 116 m<sup>2</sup> per dwelling unit.

Access is proposed at three (3) points (eastern, central, and western) on the frontage of the site. The proposed at-grade parking consists of 26 parking spaces along the frontage of Croft Street, including two (2) barrier-free spaces. Below-grade (one-storey) parking consists of 132 spaces, including four (4) barrier-free spaces. Loading spaces are proposed on the eastern and western edge of the apartment building for a total of two (2) loading spaces. An

extension of Croft Street has been proposed from the east which includes a cul-de-sac at the furthest western portion. An outdoor amenity area has been proposed on the southeastern portion of the subject site.

This report supports the OPA and ZBA required for the proposed development. The purpose of this report is to provide:

- An overview of the subject site and local area context;
- An explanation of the proposed built form, uses, and development statistics;
- A review of the applicable land use planning policy and regulatory frameworks;
- A summary of all supporting studies and technical reports; and,
- A description and justification of the proposed amendment to the Official Plan and Zoning By-law.

A pre-consultation meeting was held on October 28, 2024 to receive preliminary feedback from staff and to confirm submission requirements. This report is part of a complete application submission requirement under the Planning Act for OPA and ZBA applications. Summaries of other technical reports and studies that are being submitted as part of a complete OPA and ZBA application are provided in Section 5.0 of this Report.

## 2.0 SITE AND SURROUNDINGS

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## 2.1 Subject Site

The subject site is municipally known as 0 Croft Street and is located between Deblaquire Street North and Rose Glen Road North in the Municipality of Port Hope, Northumberland County (Refer to Figure 1). The subject lands are situated within an established residential area characterized by a mix of low and medium-density residential uses, institutional uses, and neighbourhood-scale commercial uses that serve the surrounding community.

The vacant subject site is generally irregular in shape characterized by a dense, mature tree canopy/ woodland area with a total area of approximately 1.8 hectares (4.45 acres). The lands do not presently have frontage on a constructed public right-of-way (ROW), however, frontage will be established through the proposed extension of Croft Street.

The site is not listed on the Heritage Register and is not a designated heritage site.

## 2.2 Planning History

There are no previously approved Official Plan or Zoning By-law amendments pertaining to the subject site.

## 2.3 Surrounding Land Uses

### 2.3.1 Neighbourhood Surroundings

The subject site is located within an established residential area in the Municipality of Port Hope, County of Northumberland, and more specifically along the Croft Street extension, east of Wellington Street, and between Deblaquire Street North and Rose Glen Road North. The surrounding area consists primarily of low- and medium-density residential development with planned future growth. The neighbourhood is well supported by existing services such as:

**Schools:** Beatrice Strong Public School, Port Hope High School and Trinity College.

**Parks and Open Space:** Port Hope Agricultural Park, Wladyka Park, Hewson Park and Town Park Recreation Centre.

**Commercial/Retail:** Port Hope Community Pharmacy, A-Plus Convenience, Port Hope Shopping Plaza, and nearby local service establishments.

### 2.3.2 Immediate Surroundings

The description of the immediate land uses are based on the orientation displayed on the aerial map in Figure 1. Land uses adjacent to the subject site include a mix of low-rise condominiums, low-rise residential and various neighbourhood commercial uses. Immediate surroundings within the apartment neighbourhood include:

**North:** Located directly to the north of the subject site are low-rise condominiums, the Levante Roseglen Village, low-rise residential and high-rise residential (Concord Apartments).

**West:** Located to the west of the subject site are low-rise residential uses with little neighbourhood commercial uses.

**East:** Located immediately to the east of the subject site is Wladyka Park, Beatrice Strong Public School, Calvary Pentecostal Church and low-rise residential uses.

**South:** To the immediate south of the subject site is Trinity College and low-rise residential uses.

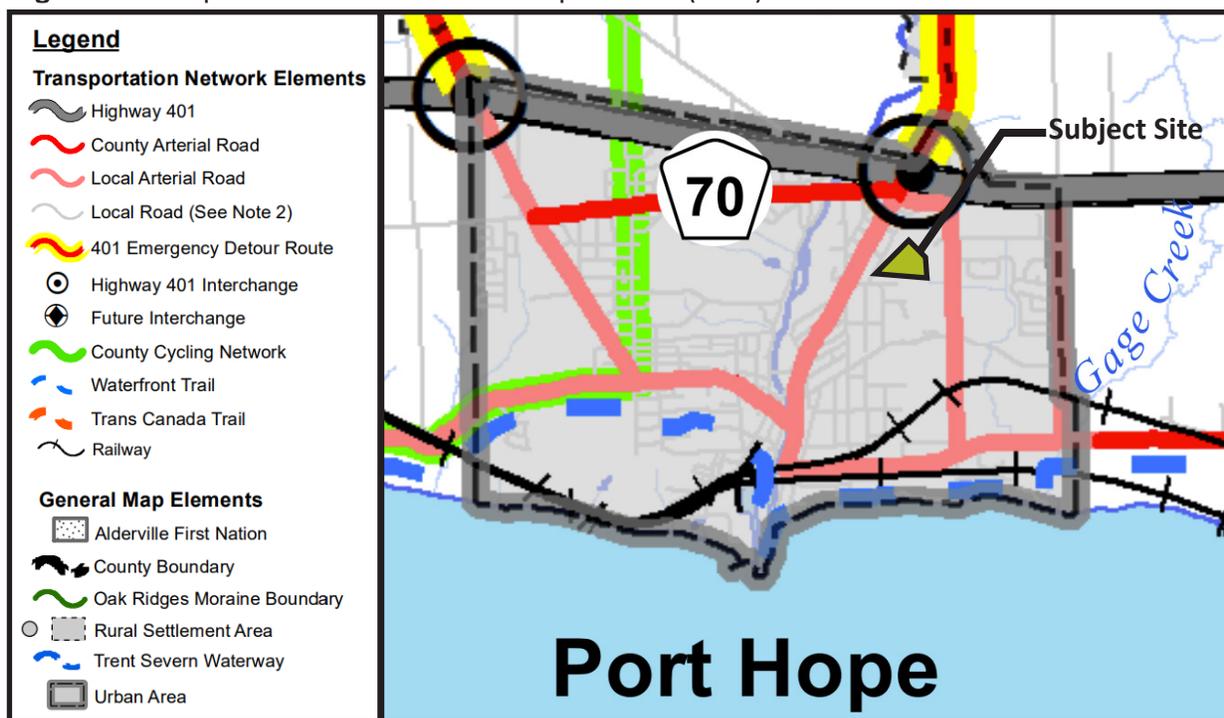
## 2.4 Transportation Network

The following subsections outline the transportation network that connects the subject site to other parts of Port Hope and the wider region.

### 2.4.1 Road Network

**Rose Glen Road North** is an arterial roadway that provides a direct connection to Highway 401 and Ontario Street (County Road 28) to the north, and to Peter Street (County Road 2) to the south, with established business districts located at both ends of the corridor. Rose Glen Road North has an urban cross-section, consisting of one (1) travel lane in each direction with an approximate lane width of 3.5 metres, a sidewalk on one side of the roadway,

**Figure 1:** Excerpt from Schedule 'C' - Transportation (NOP)



and a paved multi-use pathway on the opposite side. Schedule D1 - Transportation System Urban Area Detail of the Municipality of Port Hope Official Plan identifies Rose Glen Road North as an Arterial and a Core Active Transportation Route.

**Croft Street** is a local two-lane, two-way roadway that generally runs east to west through the subject area and is proposed to be extended west of Rose Glen Road North to support the proposed development. The Croft Street extension is planned to accommodate vehicular traffic, sidewalks, and municipal servicing consistent with Port Hope’s municipal standards. Schedule D1 - Transportation System Urban Area Detail of the Municipality of Port Hope Official Plan identifies Rose Glen Road North as a Future Collector and consists of an existing designated trail.

**Figure 2:** Excerpt from Schedule ‘D-1’ - Transportation System Urban Area Detail of the PHOP

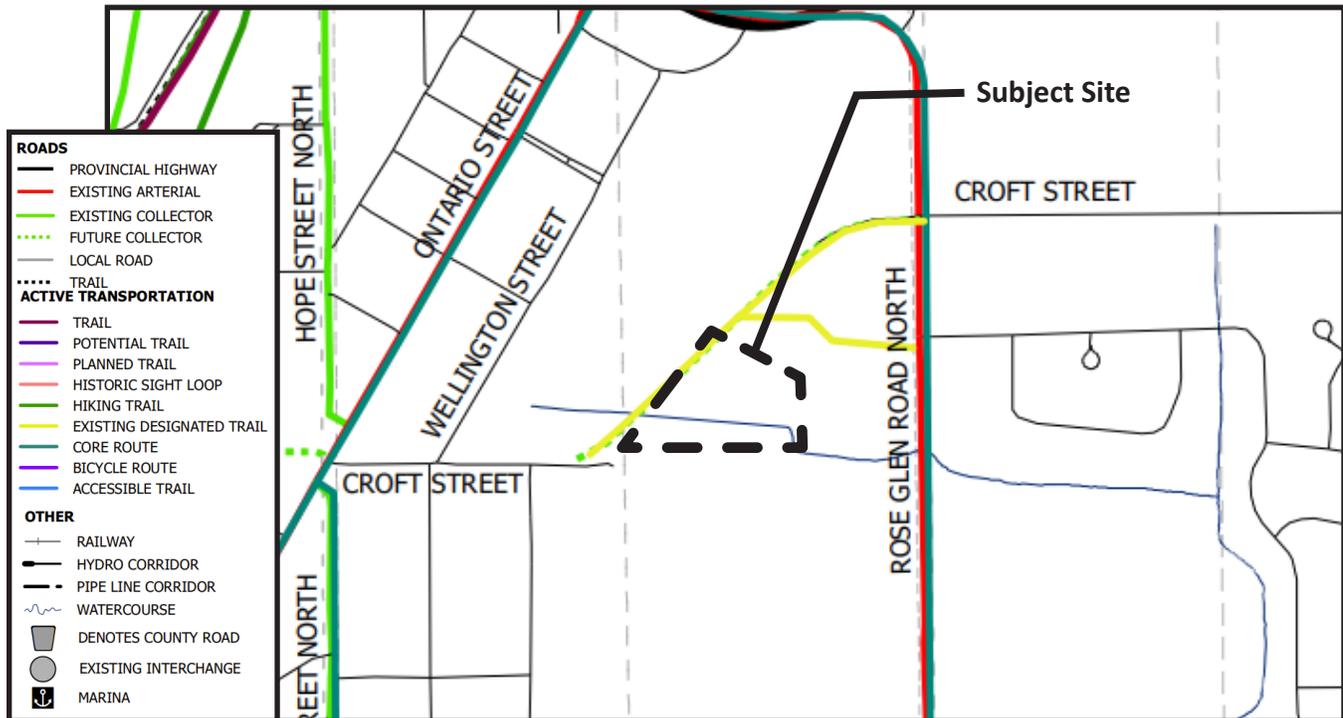




Figure 3 - Site Context Plan



## 3.0 PROPOSAL

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### 3.1 Development Overview

The proposed development is comprised of a six-storey residential apartment building consisting of two (2) towers connected via a central common lobby corridor (Refer to Figure 4: Proposed Site Plan).

The proposed development consists of a total gross floor area (GFA) of approximately 3,623.2 m<sup>2</sup> (39,000 ft<sup>2</sup>) and will accommodate a total of 108 condominium dwelling units, with 54 units in each of Building A and Building B. The common lobby will include a 1,154.2 ft<sup>2</sup> cafe. The development includes landscaped amenity space at a rate of approximately 116 m<sup>2</sup> per dwelling unit, in addition to a designated outdoor amenity area located on the southeastern portion of the subject site.

Furthermore, the proposed development includes three (3) main access points (eastern, central, and western) along the Croft Street frontage to facilitate site ingress and egress. At-grade parking is proposed along the frontage of Croft Street and consists of a total of 26 parking spaces, including two (2) barrier-free spaces. Below-grade (one-storey) parking is also proposed and consists of a total of 132 parking spaces, including four (4) barrier-free spaces.

Loading areas are proposed on the eastern and western edges of Building A and Building B, for a total of two (2) loading spaces. An extension of Croft Street is proposed from the east, which includes a cul-de-sac at the furthest western portion of Croft Street to support future access and circulation.

Landscaped areas are contemplated throughout the site, including extensive perimeter landscaping

and a centrally located outdoor amenity area in the southeastern portion of the Site. The overall site design integrates building placement, parking, access, and open space to create a functional and well-organized residential development that is compatible with the surrounding neighbourhood.

The proposed surface and underground parking will efficiently accommodate resident and visitor demand while maximizing landscaped open space across the site.

### 3.2 Required Approvals

The subject site is within the Medium Density Residential designation of the Municipality of Port Hope Official Plan (PHOP). This designation allows for townhouse, rowhouse, fourplex, low-rise apartments, and other forms of low-rise multiple dwellings.

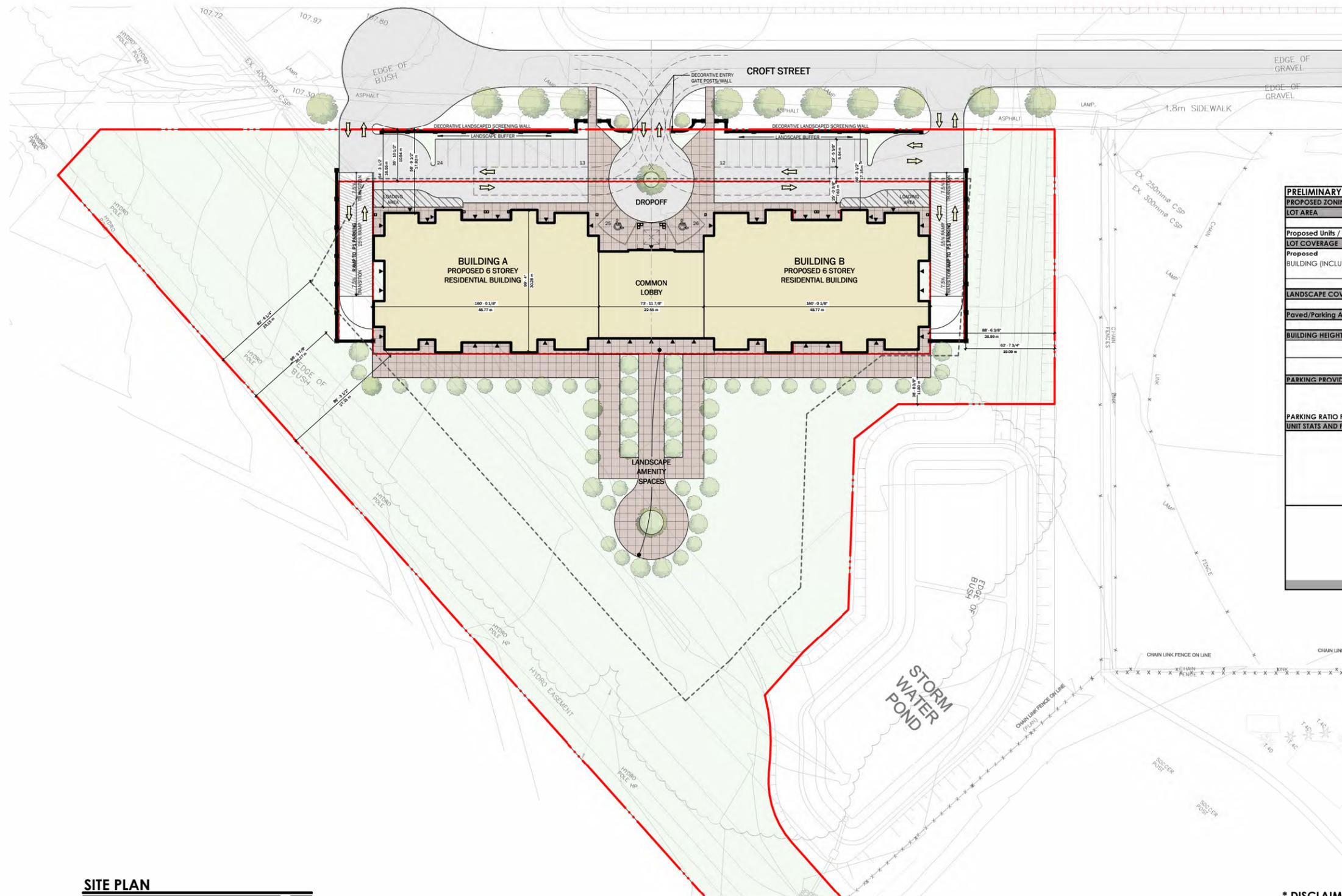
Given that the proposed development is six (6) storeys, it is not considered to be low-rise residential apartments. It has a net density of 58 units per hectare, and therefore an Official Plan Amendment is required to permit the proposed development.

An Official Plan Amendment is required to amend the subject site land use designation from Medium Density Residential to High Density Residential as per Schedule C-1 - Land Use Urban Area Detail. **Appendix X**, illustrates the proposed Official Plan Amendment.

The subject site is within Residential One-Type One Holding Symbol 1 'RES1-1(H1)' in the Municipality of Port Hope Zoning By-law. This zoning does not permit Apartment Dwelling uses.

A Zoning By-law Amendment is required to rezone the lands from Residential One – Type One Holding Symbol 1 'RES1-1(H1)' to High Density Residential 'RES4', to permit Apartment Dwelling uses. Areas of non-compliance will be required to be addressed through a site-specific exception zone. Appendix 2, illustrates the proposed Official Plan Amendment.

As per the municipality's Site Plan Control By-law 26-2008, a Site Plan Control application will be required to permit the development.



PRELIMINARY SITE STATS	HECTARES	ft <sup>2</sup>	m <sup>2</sup>	%
<b>PROPOSED ZONING - (RES4 HIGH DENSITY RESIDENTIAL)</b>				
LOT AREA	1.8581	200000.00	18,580.6	100%
Proposed Units / Hectare	58,1251			
<b>LOT COVERAGE</b>				
Proposed				
BUILDING (INCLUDES PROJECTIONS)		39000.0	3,623.2	19.5%
<b>Total</b>		<b>39000.0</b>	<b>3623.2</b>	<b>19.5%</b>
<b>LANDSCAPE COVERAGE (INCLUDES SIDEWALKS/PATIOS)</b>				
Proposed		135000.0	12,541.9	67.5%
<b>Paved/Parking Areas Not Covered By Building</b>		<b>26000.0</b>	<b>2,415.5</b>	<b>13.0%</b>
<b>BUILDING HEIGHT</b>				
Building A	6 Storey		23m (Top of Roof)	
Building B	6 Storey		23m (Top of Roof)	
<b>PARKING PROVIDED</b>				
P1 UNDERGROUND				132
Surface				26
<b>Total</b>				<b>158</b>
<b>PARKING RATIO PROVIDED</b>				
		1.46	Spaces Per Unit	
<b>UNIT STATS AND FLOOR AREAS</b>				
<b>Building A</b>	Units			
1st FLOOR	8			
2nd FLOOR	10			
3rd FLOOR	10			
4th FLOOR	10			
5th FLOOR	10			
6th FLOOR	6			
<b>Total Building A</b>	<b>54</b>			
<b>Building B</b>	Units			
1st FLOOR	8			
2nd FLOOR	10			
3rd FLOOR	10			
4th FLOOR	10			
5th FLOOR	10			
6th FLOOR	6			
<b>Total Building B</b>	<b>54</b>			
<b>Total Units Provided</b>	<b>108</b>			

**SITE PLAN**

1" = 30'-0"

\* DISCLAIMER: ISSUED FOR PRELIMINARY DESIGN DISCUSSION ONLY

# LEBLANC ENTERPRISES CROFT STREET DEVELOPMENT

CROFT STREET | PORT HOPE | ONTARIO

A · C · K

architects  
STUDIO INC.

SITE PLAN

.SP1

SCALE: AS SHOWN  
DATE: JANUARY 2024  
PROJECT No.: 2021-3475

Figure 4 - Proposed Site Plan



<b>Table 1 - Proposed Site Statistics</b>	
<b>Gross Site Area</b>	1.9 hectares
<b>Building Area</b>	3,623.2 m <sup>2</sup>
<b>Building Height</b>	23 metres (peak of roof)
<b>Site Frontage</b>	226 metres
<b>Setbacks</b>	
Front Yard	17.2 metres
Exterior Side Yard	20.3 metres
Interior Side Yard	19.1 metres
Rear Yard	11.8 metres
<b>Coverage</b>	3,623.2 m <sup>2</sup> (19.5 %)
Lot Coverage	3,623.2 m <sup>2</sup>
Soft Landscaping Coverage	12,541.9 m <sup>2</sup>
Hardscape Coverage	2,415.5 m <sup>2</sup>
<b>Vehicular Parking Spaces</b>	154
Typical Parking Space	148
Accessible Parking Space	6

## 4.0 SUPPORTING STUDIES AND REPORTS

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## 4.1 Traffic Impact Study

A Traffic Impact Study (TIS) was prepared in support of the proposed development by Jewell Engineering Inc. (Jewell). The TIS covers the surrounding neighbourhood transportation context, traffic conditions, trip distribution, traffic volumes, intersection improvements, and an overview of Transportation Demand Management (TDM).

Access to the subject site will be provided by an extension of Croft Street.

### Key Findings for the Study:

- The proposed development is anticipated to generate about 54 and 64 two-way vehicular trips during the AM and PM peak hours, respectively;
- The level of service (LOS) and vehicle queueing analysis indicates that under 2038 forecasts, the three (3) site access points and nearby intersections are expected to operate acceptably.
- All movements to and from the site will continue to provide good levels of service.

### Key Findings for Parking Spaces:

The Municipality of Port Hope requires a total of 147 parking spaces to be included in the proposed development, consisting of 108 parking spaces, 27 visitor parking spaces and 12 dedicated restaurant parking spaces.

The proposed development consists of a total of 154 parking spaces, comprised of 26 at-grade parking spaces including two (2) barrier-free spaces and a single-floor underground parking area consisting of 132 parking spaces, including four (4) barrier-free

spaces. No visitor parking spaces have been specified and no dedicated parking spaces are provided for the cafe located in the common amenity space.

TDM is the application of strategies and policies to reduce travel demand, or to redistribute this demand in space or time. Demand, when managed can be a cost-effective alternative to increasing capacity. TDM in the study area has been determined and no further improvements are proposed.

Loading spaces are proposed on the eastern and western edge of Tower A and Tower B, for a total of two (2) loading spaces. The vehicle movement analysis concluded that passenger vehicles can safely maneuver within the parking areas, and service vehicles can safely access the site and loading areas.

The Traffic Impact Study, prepared under separate cover provides more detail to what has been briefly summarized above.

## 4.2 Functional Servicing Report & Preliminary Stormwater Management Report

A Functional Servicing and Stormwater Management Report was prepared in support of the proposed development by Jewell Engineering Inc. (Jewell).

### Recommendations (Functional Servicing Report):

#### Conclusion #1 – Water Supply

*A 200mm watermain on Croft St with 150mm stub to the development property will be installed as part of the Croft St Extension work. Under peak hour conditions, the proposed development will experience a minimal pressure loss, and will operate within MOE operating pressures. The building will be sprinklered to provide adequate fire protection and sufficient fire flow is available in the system to service the proposed development. In conclusion, adequate pressure and capacity is available in the municipal system to service the proposed development.*

#### Conclusion #2 – Sanitary Servicing

*A 200 mm sanitary sewer at a minimum slope of 0.4% will be adequate to provide sanitary conveyance for the development. The downstream sewer system on Croft Street has sufficient capacity to accommodate the proposed development to the trunk sewer on Rose Glen Road North.*

### Key Findings (Stormwater Management Report):

Jewell reviewed the proposed wet pond and determined its revised configuration offer improved land efficiency for the site while

accommodating increased rainfall with current intensity-duration-frequency (IDF) curves.

The revised wet pond meets quality treatment objectives by removing >80% TSS loading from the site and Croft Street condominium to the north.

The revised wet pond meets quantity control objectives by meeting the flow control targets set in the 2007 West Gage Creek Master Drainage Plan with increased rainfall depth values and further tested with a standard pre-to-post objectives.

The proposed Stormwater Management plan and preliminary grading plan, confirms the revised wet pond design meets the Stormwater Management objectives established in Section 1.2 of the Preliminary Stormwater Management Report.

Jewell concludes the proposed development is serviceable in accordance with the above findings. It is the recommendation of Jewell that the proposed development may proceed, subject to the implementation of the servicing requirements outlined in the Functional Servicing Report and Preliminary Stormwater Management Report. The Functional Servicing Report and Preliminary Stormwater Management Report, prepared under separate cover provides more detail to what has been briefly summarized above.

### 4.3 Geotechnical Investigation Report

A Geotechnical Investigation Report in support of the proposed development was prepared by Geo-Logic Inc. The report outlines the subsurface conditions and engineering recommendations necessary for the construction of the proposed six-storey residential apartment building and single-storey underground parking level.

#### Key Findings:

The subsurface conditions typically encountered a surficial layer of topsoil, over native till ranging from 100 to 200 mm. The soil was observed to be in a damp, loose state with a silty, highly organic content. The till generally appeared brown in colour consisting of clayey silty sand to sandy silty sand and varying amounts of gravel. The soil was typically in a moist to wet and compact to very dense in-situ state, with the density generally increasing with depth.

Groundwater operations in several test pits encountered water seepage ranging from a depth of 0.0 to 2.4 mbeg. It is expected further excavations will result in groundwater seepage, and if any excavations require more intensive de-watering / control, the use of filtered sumps, or other suitable method of de-watering and/or sheet piling may be appropriate.

Foundation designs recommend using shallow and deep foundations based on existing soil conditions, with specific bearing capacities and precautions to minimize settlement. In addition, recommendations for site preparation, include the removal of unsuitable materials and engineered fill placement.

Slab-on-grade construction requires moisture barriers and proper compaction to prevent cracking and settlement. Pavement structures are proposed for light and heavy duty use with specifications for optimal durability. Construction monitoring is advised to ensure compliance with geotechnical recommendations, and limitations of the findings underscore the need for adjustments if site conditions or designs deviate from the investigation.

It is the recommendation of Geo-Logic that the proposed development may proceed, subject to the subsurface measures outlined in the Geotechnical Investigation Report.

The Geotechnical Investigation Report, prepared under separate cover provides more detail to what has been briefly summarized above.

#### 4.4 Environmental Impact Study

An Environmental Impact Study (EIS) in support of the proposed development was prepared by Cambium Inc. (Cambium) and it serves to address potential impacts to natural heritage features on site.

##### Key Findings:

The site contains two ephemeral watercourses that convey seasonal runoff and storm flows to West Gages Creek. These features are poorly defined, lack permanent flow, and provide limited aquatic habitat function. No fish habitat was identified on-site.

Provincial mapping previously identified unevaluated wetlands across much of the site; however, detailed field investigations and soil sampling confirmed that the majority of these areas do not meet wetland criteria. A small wetland is confirmed in the southeast corner of the site. No Provincially Significant Wetlands (PSWs) are present on-site.

No significant woodlands, significant valleylands, or Areas of Natural and Scientific Interest were identified on or adjacent to the site.

Wildlife habitat on the site supports primarily common species. Breeding bird surveys confirmed breeding by four common species. Chimney Swift (Special Concern) was observed foraging in the area but no suitable nesting habitat exists on the site.

Two Butternut trees (Endangered) were identified along the western edge of the site.

These trees are subject to protection under the Endangered Species Act and will require appropriate setbacks, permitting, and mitigation measures from impacts of the proposed development.

With implementation of recommended avoidance measures, mitigation strategies, and best management practices, the proposed development is not anticipated to result in negative impacts to significant natural heritage features or their ecological functions.

It is the recommendation of Cambium that the proposed development may proceed, subject to the implementation of the mitigation measures and regulatory requirements outlined in the Environmental Impact Study.

The Environmental Impact Study, prepared under separate cover provides more detail to what has been briefly summarized above.

## 5.0 POLICY AND REGULATORY CONTEXT

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The following sub-sections provide an overview of Provincial, Regional, and Municipal planning policies that inform development on the subject site. This analysis outlines how the proposed development is consistent with the Provincial Planning Statement, conforms to the County of Northumberland Official Plan and conforms with the direction and policy framework of the Municipality of Port Hope Official Plan.

### 5.1 Planning Act R.S.O. 1990, c. P.13

The Planning Act R.S.O. 1990, c. P.13 (Planning Act) is enabling provincial legislation that establishes the framework for land use planning in Ontario. The Planning Act provides for the integration of provincial matters in planning decisions in a fair, accessible and organized process. It encourages coordination between various interest groups, and provides accountability for decision-making authorities within the planning process.

The Planning Act requires that all planning decisions shall “have regard to” matters of Provincial interest as identified in Section 2. Matters that apply to the proposed development include:

*(f) the adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;*

*(g) the minimization of waste;*

*(h) the orderly development of safe and healthy communities;*

*(k) the adequate provision of employment opportunities;*

*(p) the appropriate location of growth and development;*

*(q) the promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians;*

*(r) the promotion of built form that is well designed, encourages a sense of place, and provides for public spaces that are of high quality, safe, accessible, attractive and vibrant.*

Section 3(5) of the Planning Act states that planning decisions must be consistent with the policy statements and conform with provincial plans that are in effect on that date.

**It is of our professional planning opinion, the proposed development of the subject site is consistent with the Planning Act and has regard to various matters of provincial interest. The site is an appropriate location for growth and, given its size, can comfortably accommodate intensification. The proposal will contribute to the supply of housing by diversifying options, optimizes the use of infrastructure, and achieves well-designed built form that fits appropriately within the surrounding context. This is further analyzed below.**

## 5.2 Provincial Planning Statement (2024)

The Provincial Planning Statement (PPS) 2024 is a policy document that establishes a planning and land use framework on matters of provincial interest applicable across Ontario. The PPS 2024 came into effect on October 20, 2024, replacing both the Provincial Policy Statement (2020) and the Growth Plan for the Greater Golden Horseshoe (2019). The Planning Act requires all decisions affecting planning matters to be consistent with the PPS.

Chapter 1 of the PPS outlines the vision, where it emphasizes to increase the housing supply, focus growth within urban and rural settlement areas, prioritize compact, transit-oriented design and optimize the use of existing infrastructure and public service facilities to support long-term prosperity.

Chapter 2 of the PPS provides a number of policies that support strong and competitive communities.

### Chapter 2: Building Homes, Sustaining Strong and Competitive Communities

#### 2.1 Planning for People and Homes

**Policy 2.1.4:** *To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:*

*a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through lands which are designated and available for residential development; and*

*b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans.*

#### **Policy 2.1.4(a-b) Analysis:**

With respect to the County of Northumberland and the Municipality of Port Hope, a long-term supply of designated and serviced residential land contributes to the development of housing. The proposed development positively contributes to this policy direction with a six-storey apartment building, comprised of a total of 108 condominium units. The proposal represents intensification that supports both the County's and Municipality's ability to meet projected residential growth targets. The proposed development improves an underutilized and vacant parcel of land, therefore supporting the objectives of a 15-year supply of designated residential land and the 3-year supply of serviced land required for development.

The proposed development contributes to growth and is supported through improved municipal services and increased access to available housing options.

**Policy 2.1.6:** *Planning authorities should support the achievement of complete communities by:*

*a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities,*

*longterm care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;*

*b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and*

*c) improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.*

**Policy 2.1.6(a-c) Analysis:**

The proposed development contributes to providing an increase in housing through a six-storey apartment building, comprised of a total of 108 condominium units. The site is located near schools, commercial uses, public service facilities, employment areas and recreational amenities, integrating the proposed density into an area that is supported by multiple land uses. The extension of Croft Street and associated infrastructure improvements also enhance multimodal transportation options.

The proposed development enhances accessibility for people of varying ages and abilities, and reduces barriers to accessing housing by providing a mid-rise typology located within an established residential neighbourhood and has regard for barrier-free accessibility standards. In addition, the proposed dwelling units will be designed and built in accordance with the Ontario Building Code and applicable Accessibility Requirements.

With respect to the housing options proposed, it will increase access to housing, improve social equity and quality of life. The proposal supports the achievement of complete communities as outlined in PPS Policy 2.1.6.

**2.2 Housing**

**Policy 2.2.1:** *Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:*

*a) establishing and implementing minimum targets for the provision of housing that is affordable to low and moderate income households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs;*

*b) permitting and facilitating:*

*1. all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and*

*2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of*

*new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;*

*c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and*

*d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.*

**Policy 2.2.1(a-d) Analysis:**

The proposed development consists of a six-storey apartment building, comprised of a total of 108 condominium units which contributes to the overall supply of housing within the area, diversifies the housing typology and responds to demographic and social needs making it suitable for a variety of households. By increasing the total number of available units, it supports broader affordability objectives by expanding options.

The proposed development promotes efficient use of land by providing 108 dwelling units within a total building footprint of approximately 1.9 hectares (ha), resulting in a net density of 59 units per hectare. The proposed development is located within an established residential neighbourhood that has planned road extensions, improving multimodal connectivity options and supports transit-supportive land use patterns.

**2.3 Settlement Areas and Settlement Area Boundary Expansions**

**Policy 2.3.1.1:** *Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.*

**Policy 2.3.1.2:** *Land use patterns within settlement areas should be based on densities and a mix of land uses which:*

- a) efficiently use of land and resources;*
- b) optimize existing and planned infrastructure and public service facilities;*
- c) support active transportation*
- d) are transit-supportive, as appropriate;*

**Policy 2.3.1.1 and 2.3.1.2 (a-d) Analysis:**

The site is identified as a settlement area as defined within the PPS (2024). It is designated Urban Area on Schedule 'A' - Land Use of the County of Northumberland Official Plan (NOP) and Residential - Medium Density on Schedule 'C1' - Land Use of the Municipality of Port Hope Official Plan (PHOP). In addition, the site is within a designated 'Major Intensification Area' on Schedule 'A-1' on PHOP, and as such is within a growth area that is subject to the density target of 50 residents and jobs per gross hectare per the policies of the PPS.

The proposed development exceeds this minimum density target and provides for 59 residents and jobs per gross hectare. The site

represents a logical progression for development east to west and is supported by the extension of Croft Street Watermain/Sanitary Sewer Trunk Line (Refer to Port Hope Asset Management Plan, 2016). With planned road extensions, improving lateral connectivities, it also provides multimodal connectivity options and supports transit-supportive land use patterns. The proposed development will contribute to providing growth within settlement areas.

## 2.4 Strategic Growth Areas

**Policy 2.4.1.1:** *Planning authorities are encouraged to identify and focus growth and development in strategic growth areas.*

### Policy 2.4.1.1 Analysis:

The site is located within the Urban Area on Schedule 'A' - Land Use of the County of Northumberland Official Plan (NOP) and Residential - Medium Density on Schedule 'C1' - Land Use of the Municipality of Port Hope Official Plan (PHOP). In addition, the site is within a designated 'Major Intensification Area' on Schedule 'A-1' on PHOP, which functions as a Strategic Growth Area (SGA) as defined by the PPS.

Croft Street (west of the site) is a Local Road and Rose Glen Road North (east of the site) is an Existing Arterial Road as per Schedule D (Transportation System) of the PHOP. The site fronts on Croft Street where it is Croft Road is identified as Existing Designated Trail and Future Collector as per Schedule D-1 (Transportation System) of the PHOP.

Strategic growth areas are characterized to be within settlement areas, nodes, corridors, and other areas that have been identified by municipalities to be the focus for accommodating intensification and higher density mixed uses in a more compact built form. Lands along major roads, arterials, or other areas with existing or planned frequent transit service or higher order transit corridors may also be identified as strategic growth areas.

The subject site is intended to accommodate a share of population growth through higher-density development supported by municipal services and planned infrastructure improvements.

Therefore, the proposed six-storey apartment building comprised of a total of 108 condominium units contributes to the overall supply of housing within the area. It diversifies the housing options and supports broader growth objectives reinforcing the broader intensification and growth strategy.

**Policy 2.4.1.2:** *To support the achievement of complete communities, a range and mix of housing options, intensification and more mixed-use development, strategic growth areas should be planned:*

- a) to accommodate significant population and employment growth;*
- b) as focal areas for education, commercial, recreational, and cultural uses;*
- c) to accommodate and support the transit network and provide connection points for inter-*

*and intra-regional transit; and*

*d) to support affordable, accessible, and equitable housing.*

**Policy 2.4.1.2(a-d) Analysis:**

Strategic Growth Areas (SGAs) are planned to support complete communities, higher-density housing, transit-supportive design, and mixed-use opportunities. The proposed development introduces higher-density residential housing within a designated growth area, by providing 108 dwelling units (59 units/hectare), contributing meaningfully to the municipality's population growth targets. The proposal supports compact intensification on a vacant site and aligns with County and local Official Plan directions for growth to be within SGAs.

Strategic Growth Areas are intended to support a variety of land uses. The addition of 108 condominium dwelling units strengthens the area for community activity and supports the viability of existing commercial, recreational, and institutional uses.

The proposed development is located on vacant land within an established neighbourhood and can be supported by existing and planned municipal services such as the Croft Street Watermain/Sanitary Sewer Trunk Line (Refer to Port Hope Asset Management Plan, 2016) and Future Collector as per Schedule D-1 (Transportation System) of the PHOP. The extension of Croft Street improves lateral connectivity and reinforces the long-term transportation network envisioned by the municipality.

The proposed dwelling units are market-based, where affordable housing have not been disclosed as of November 2025, however, the proposed development contributes to the overall housing supply, which supports affordability across the housing market. The higher-density, multi-unit built form provides an accessible housing option for a range of household types, including smaller households, seniors, and those seeking attainable ownership opportunities.

The proposed development directs significant residential growth to a designated Strategic Growth Area, enhances community function, and contributes to a more accessible and varied housing supply.

**2.9 Energy Conservation, Air Quality and Climate Change**

**Policy 2.9.1:** *Planning authorities shall plan to reduce greenhouse gas emissions and prepare for the impacts of a changing climate through approaches that:*

*a) support the achievement of compact, transit-supportive, and complete communities;*

*b) incorporate climate change considerations in planning for and the development of infrastructure, including stormwater management systems, and public service facilities;*

*c) support energy conservation and efficiency;*

*d) promote green infrastructure, low impact development, and active transportation,*

*protect the environment and improve air quality; and*

*e) take into consideration any additional approaches that help reduce greenhouse gas emissions and build community resilience to the impacts of a changing climate.*

**Policy 2.9.1(a-e) Analysis:**

The proposed development and design establish a framework and servicing strategy that will support energy conservation, air quality and climate change objectives through compact development, the efficient use of infrastructure, encouragement of multi-modes of transportation and transit, and the preservation and enhancement of natural features and functions.

Chapter 3 of the PPS provides a number of policies that support infrastructure and public services.

**Chapter 3: Infrastructure and Facilities**

**3.1 General Policies for Infrastructure and Public Service Facilities**

**Policy 3.1.1:** *Infrastructure and public service facilities shall be provided in an efficient manner while accommodating projected needs.*

*Planning for infrastructure and public service facilities shall be coordinated and integrated with land use planning and growth management so that they:*

*a) are financially viable over their life cycle, which may be demonstrated through asset management planning;*

*b) leverage the capacity of development proponents, where appropriate; and*

*c) are available to meet current and projected needs.*

**Policy 3.1.1(a-c) Analysis:**

The proposed development will be supported by the planned extension of Croft Street, including water and sanitary sewer infrastructure identified in the Port Hope Asset Management Plan (2016). Extending existing trunk services to support intensification within a designated growth area represents a financially viable servicing approach, maximizing prior municipal investment and reducing long-term lifecycle costs by directing development to where servicing capacity can be efficiently provided.

The development proposal leverages the proponent’s ability to construct the required road extension, internal private infrastructure, and lateral servicing connections.

With the extension of water, wastewater, and road infrastructure to a vacant and strategically located parcel within the Urban Area and Residential - Medium Density designations (NOP and PHOP), the proposal ensures that these services will be available to meet both current and projected residential needs. The proposed development consists of 108 condominium units which supports Port Hope’s broader intensification strategy and contributes to the required growth within the settlement area. Planned transportation improvements also enhance multimodal connectivity, supporting active and transit-supportive mobility options consistent with long-term community needs.

**Policy 3.1.2:** *Before consideration is given to developing new infrastructure and public service facilities:*

*a) the use of existing infrastructure and public service facilities should be optimized;*

**Policy 3.1.2(a) Analysis:**

The proposed development and design establish a framework and servicing strategy that will support energy conservation, air quality and climate change objectives through compact development, the efficient use of infrastructure, encouragement of multi-modes of transportation and transit, and the preservation and enhancement of natural features and functions.

**3.6 Sewage, Water and Stormwater**

**Policy 3.6.1:** *Planning for sewage and water services shall:*

*a) accommodate forecasted growth in a timely manner that promotes the efficient use and optimization of existing municipal sewage services and municipal water services and existing private communal sewage services and private communal water services;*

*b) ensure that these services are provided in a manner that:*

*1. can be sustained by the water resources upon which such services rely;*

*2. is feasible and financially viable over their life cycle;*

*3. protects human health and safety, and the natural environment, including the quality and quantity of water; and*

*4. aligns with comprehensive municipal planning for these services, where applicable.*

*c) promote water and energy conservation and efficiency;*

*d) integrate servicing and land use considerations at all stages of the planning process;*

*e) consider opportunities to allocate, and re-allocate if necessary, the unused system capacity of municipal water services and municipal sewage services to support efficient use of these services to meet current and projected needs for increased housing supply; and*

*f) be in accordance with the servicing options outlined through policies 3.6.2, 3.6.3, 3.6.4 and 3.6.5.*

**Policy 3.6.2:** *Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.*

**Policy 3.6.1(a-f) and Policy 3.6.2 Analysis:**

The proposed development accommodates forecasted growth within a designated Urban

Area, Residential - Medium Density and Major Intensification Area. The extension of the Croft Street Watermain/Sanitary Sewer Trunk Line identified in the Port Hope Asset Management Plan (2016), enables the optimization of existing servicing systems and supports growth in an efficient, coordinated manner.

The site will be serviced by municipal water and wastewater systems that operate within established and planned capacity ensuring long-term sustainability of local water resources and reducing the risks to public health.

The Croft Street servicing extension is within a designated growth area and is consistent with Port Hope’s long-term servicing strategy which represents a financially viable system.

Through site-specific building infrastructure in accordance with OBC Standards, the proposal can integrate water-efficient fixtures and energy efficient systems to reduce overall per-capita water consumption and support the overall Town’s direction on conservation.

Overall, the proposed development is consist with the PPS Policy 3.6.1 and Policy 3.6.2, as it coordinates growth with efficient servicing, supports long-term sustainability, and effectively utilizes existing and planned infrastructure to meet current and future projected housing needs.

**Policy 3.6.8:** *Planning for stormwater management shall:*

*a) be integrated with planning for sewage and water services and ensure that systems*

*are optimized, retrofitted as appropriate, feasible and financially viable over their full life cycle;*

*b) minimize, or, where possible, prevent or reduce increases in stormwater volumes and contaminant loads;*

*c) minimize erosion and changes in water balance including through the use of green infrastructure;*

*d) mitigate risks to human health, safety, property and the environment;*

*e) maximize the extent and function of vegetative and pervious surfaces;*

*f) promote best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development; and*

*g) align with any comprehensive municipal plans for stormwater management that consider cumulative impacts of stormwater from development on a watershed scale.*

**Policy 3.6.8(a-g) Analysis:**

The proposed development aligns with the the extension of the Croft Street Watermain/ Sanitary Sewer Trunk Line identified in the Port Hope Asset Management Plan (2016), which enables the optimization of existing servicing systems and supports growth in an efficient, coordinated manner that is financially viable over its life cycle.

The proposed development will include stormwater controls that limit post-development run-off, manage peak flows, and reduce contaminant loads through on-site treatment measures. This mitigates impacts on downstream systems.

The site will be serviced by municipal water and wastewater systems that operate within established and planned capacity ensuring long-term sustainability of local water resources and reducing the risks to public health. Cambium Inc., has prepared an Environmental Impact Study outlining Mitigation Measures and Best Management Practices Recommendations to support stormwater infiltration, minimize erosion and sedimentation, protect on-site watercourses, and mitigate flooding and erosion hazards. This protects public safety, private property, and environmentally sensitive features.

The proposed development maximizes landscaped areas, tree planting, and permeable surfaces that increase infiltration capacity. It will consist of approx. 12,542 m<sup>2</sup> (67.5 %) of landscaped area further enhancing overall stormwater performance.

Stormwater management practices will include on-site flow attenuation, low-impact development (LID) and water conservation practices for efficient water use. These features reduce reliance on municipal stormwater systems and support natural groundwater recharge systems.

Through site-specific building infrastructure in accordance with OBC Standards, the proposal can integrate water-efficient fixtures and energy

efficient systems to reduce overall per-capita water consumption and support Port Hope's direction on conservation.

Overall, the proposed development is consistent with the PPS Policy 3.6.1 and Policy 3.6.2, as it coordinates growth with efficient servicing, supports long-term sustainability, and effectively utilizes existing and planned infrastructure to meet current and future projected housing needs.

## Chapter 4: Wise Use and Management of Resources

### 4.1 Natural Heritage

**Policy 4.1.3:** *Natural heritage systems shall be identified in Ecoregions 6E & 7E, recognizing that natural heritage systems will vary in size and form in settlement areas, rural areas, and prime agricultural areas.*

#### **Policy 4.1.3 Analysis:**

The subject site is situated in the Lake Simcoe Rideau Ecoregion 6E. Cambium Inc., has prepared an Environmental Impact Study with reference to the Natural Heritage Reference Manual (NHRM) where Woodland Significance and Areas of Natural and Scientific Interest (ANSI) are determined to not meet significant thresholds and are not present.

**Policy 4.1.4:** *Development and site alteration shall not be permitted in:*

*a) significant wetlands in Ecoregions 5E, 6E and 7E;*

**Policy 4.1.5:** *Development and site alteration shall not be permitted in:*

*a) significant wetlands in the Canadian Shield north of Ecoregions 5E, 6E and 7E;*

*b) significant woodlands in Ecoregions 6E and 7E (excluding islands in Lake Huron and the St. Marys River);*

*d) significant wildlife habitat;*

*e) significant areas of natural and scientific interest; and*

*unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions.*

**Policy 4.1.4(a) and Policy 4.1.5 (a,b,d,e)  
Analysis:**

The subject site is situated in the Lake Simcoe Rideau Ecoregion 6E. The MNR mapping initially indicated the presence of a Provincially Significant Wetland (PSW) covering the majority of the site, however in the Environmental Impact Study (EIS) prepared by Cambium Inc., field surveys were conducted on April 5, 2022, and September 9, 2022 confirmed the extent of wetland boundaries to the southeastern portion of the site.

In addition, a Wetland Impact Statement, dated August 8, 2023 and discussions with GRCA concluded that the majority of mapped PSW did not meet the criteria for provincial significance. The soil samples collected as part of the evaluation further corroborated these findings

and resulted in updated provincial mapping.

The proposed development footprint is located outside of the field verified wetland on site. The EIS outlines recommendations for the development to ensure local hydrological and ecological function is maintained.

**Policy 4.1.6:** *Development and site alteration shall not be permitted in fish habitat except in accordance with provincial and federal requirements.*

**Policy 4.1.6 Analysis:**

Cambium Inc., has prepared an Environmental Impact Study and has detailed that the watercourses on site do not provide consistent channel/bank structure to support fish, and are not considered direct fish habitat. Fish were not observed on or adjacent to the Site during any of the field investigation and therefore Policy 4.6 is not applicable.

**Policy 4.1.7:** *Development and site alteration shall not be permitted in habitat of endangered species and threatened species, except in accordance with provincial and federal requirements.*

**Policy 4.1.7 Analysis:**

Endangered and threatened species that are regionally identified to be present, include the Chimney Swift. The Chimney Swift was observed foraging in the area, however, no significant evidence of nesting or roosting structures were present on-site, meaning the lands do not contain regulated Chimney Swift habitat.

Various bat species were screened including the following:

- Eastern Small-footed Myotis
- Little Brown Myotis
- Northern Myotis
- Tri-coloured Bat
- Adjacent Special Concern Species

Screening for endangered and threatened bat species identified a small number of cavity trees, but not at a density capable of supporting roosting habitats. The site is observed as an opportunity for foraging. No evidence of SAR bats were observed and the EIS recommends the proposed development to ensure measures are taken to minimize impact during nesting season from March 15 to November 30 to mitigate any impact to potential roosting bats.

Two (2) Butternut trees were identified, but both were assessed as Category 1 under O. Reg. 830/21, indicating advanced infection and exempting them from the Endangered Species Act and its protection.

Based on the EIS findings, the site does not contain the regulated habitat of any endangered or threatened species, and the proposed development fully complies with PPS Policy 4.1.7.

## 4.2 Water

**Policy 4.2.1:** *Planning authorities shall protect, improve or restore the quality and quantity of water by:*

*a) using the watershed as the ecologically meaningful scale for integrated and long-*

*term planning, which can be a foundation for considering cumulative impacts of development;*

*b) minimizing potential negative impacts, including cross-jurisdictional and crosswatershed impacts;*

*c) identifying water resource systems;*

*d) maintaining linkages and functions of water resource systems;*

*e) implementing necessary restrictions on development and site alteration to:*

*1. protect all municipal drinking water supplies and designated vulnerable areas; and*

*2. protect, improve or restore vulnerable surface and ground water, and their hydrologic functions;*

*f) planning for efficient and sustainable use of water resources, through practices for water conservation and sustaining water quality;*

### **Policy 4.2.1 (a-f) Analysis:**

The subject site consists of two (2) ephemeral watercourses (north and south channels) and a small wetland on the south-east corner. The EIS confirms these channels are poorly functioning, seasonally active only during snowmelt or storm events, and ultimately drain 1.6 km southeast toward West Gage Creek.

The stormwater and grading design considers these watercourses by managing flows to prevent increased discharge to these channels and by mitigating cumulative downstream impacts. The proposed development will require channel re-alignment and native vegetation plantings as detailed in the EIS, which will improve hydrologic and ecological functions on the site.

Given their limited hydrologic function (shallow, poorly defined banks, minimal flow, debris accumulation), the proposed measures ensure no worsening of existing conditions and minimize cross-watershed effects.

The EIS identifies the north and south watercourse channels as components of the water resource system. The condition of the channels have been assessed and the re-alignment is designed to ensure measures are incorporated for erosion, and destabilization.

The EIS confirms no municipal drinking water, wellhead protection areas, or designated vulnerable areas occur on or near the site. The channels do not provide direct potable water, and as a result, the proposed development poses no risk to drinking water sources and integrates measures to protect surface water quality.

**Policy 4.2.5:** *All municipalities undertaking watershed planning are encouraged to collaborate with applicable conservation authorities.*

**Policy 4.2.5 Analysis:**

The EIS and stormwater management measures represents co-ordination between the Municipality of Port Hope and the Ganaraska

Region Conservation Authority (GRCA). The hydrologic assessment of the two (2) ephemeral watercourse channels, their limited function, and the proposed mitigation measures were undertaken in accordance with GRCA guidance.

**4.6 Cultural Heritage and Archaeology**

**Policy 4.6.2:** *Planning authorities shall not permit development and site alteration on lands containing archaeological resources or areas of archaeological potential unless the significant archaeological resources have been conserved.*

**Policy 4.6.2 Analysis:**

Ironstone Archaeology Inc. has prepared a Stage 1 and Stage 2 Archaeological Assessment to determine the archaeological potential of the site, primarily due to its proximity (within 300 m) to water resources and its potential to recover Indigenous resources. The Stage 1 Archaeological Assessment conducted on November 25, 2024 concludes the site retains Cultural Heritage Value or Interest (CHVI) and therefore requires a Stage 2 test-pit assessment. On May 6, 2025, a Stage 2 Archaeological Assessment was completed and no archaeological resources were found that require further assessment or mitigation. Therefore, archaeological concerns regarding have been appropriately addressed and conserved through the assessment process.

### Summary of PPS Analysis

It is our professional planning opinion that the proposed development is consistent with the PPS. The site is located within the Urban Area on Schedule 'A' - Land Use of the County of Northumberland Official Plan (NOP) and Residential - Medium Density on Schedule 'C1' - Land Use of the Municipality of Port Hope Official Plan (PHOP). In addition, the site is within a designated 'Major Intensification Area' on Schedule 'A-1' on PHOP, which functions as a Strategic Growth Area (SGA) as defined by the PPS.

The proposal contemplates a six-storey apartment buildings, comprised of a total of 108 condominium units contributing to the overall supply of housing within the area. It diversifies the housing options and supports broader growth objectives reinforcing the broader intensification and growth strategy.

Overall, the proposed development is consistent with the PPS, as it co-ordinates growth with efficient servicing, supports long-term sustainability, and effectively utilizes existing and planned infrastructure to meet current and future projected housing needs.

### 5.3 County of Northumberland Official Plan (2016)

The County of Northumberland Official Plan (NOP) is a long-term regional policy document that establishes planning and land use framework on matters of public interest applicable across the County, including guiding growth. The County of Northumberland Official Plan was adopted by Council on September 17, 2014 and approved by the Ontario Municipal Board on November 23, 2016.

The site is designated as follows under the County Official Plan and the various policies apply:

#### Section B: Growth Management

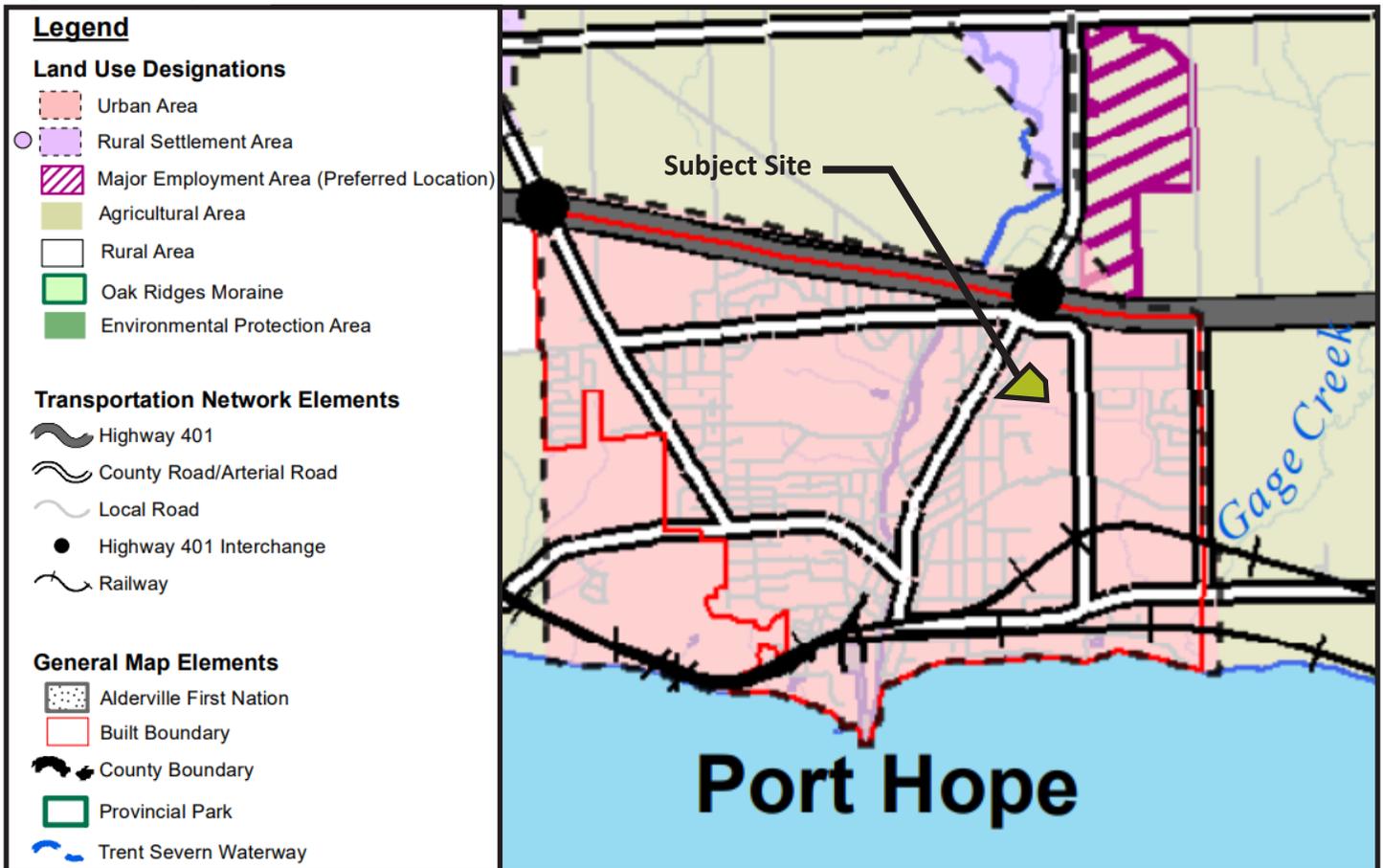
##### B1 Urban Areas / Rural Settlement Areas

**Policy B1:** *Urban areas and rural settlement areas shall be the focus of growth and their vitality and regeneration shall be promoted.*

##### Policy B1 Analysis:

The site is located within the Urban Area

Figure 5: Excerpt from Schedule 'A' - Land Use (NOP)



designation on Schedule 'A' - Land Use of the County of Northumberland Official Plan (NOP) which describes settlement areas in the County that have built boundaries established and are the focus of future growth and development. Urban Areas will support intensification, higher density mixed uses in a more compact built form and are typically located along major roads, arterials, or other areas with existing or planned infrastructure.

The proposed development positively contributes to this policy direction with a six-storey apartment building, comprised of a total of 108 condominium units. The proposal represents intensification that supports the County's direction of growth within urban areas by improving an underutilized and vacant parcel of land to increase housing supply.

## **B2 Urban Areas**

**Policy B2:** *A minimum of 80% of expected population and employment in the planning period is expected to occur in the six urban areas in the County as shown on Tables A and B. As the planning period for this Official Plan ends in 2034, the population and employment forecasts for 2034 apply. The forecasts for 2036 and 2041 shown on Tables A and B are included for planning purposes. However, these numbers extend beyond the planning period and cannot be relied upon to determine land needs beyond 2034.*

### **Policy B1 and Policy B2 Analysis:**

The County Official Plan designates the subject site as 'Urban Area' on Schedule 'A' - Land Use. The 'Urban Area' designation is intended to

be the focus of growth within the County with greater specificity of the range of permitted uses.

A minimum of 80% of the anticipated population and employment is expected to occur in the six *urban areas* in the County, one of which is the Municipality of Port Hope where the site is located. Table A provides population growth forecasts up to the year 2041 of 6,290 persons in the Municipality of Port Hope, which represents the second most significant growth within Northumberland County. Table B provides an employment growth forecast up to the year 2041 of 1,729 jobs in the Municipality of Port Hope, which represents the second most significant growth within Northumberland County.

The proposal contemplates a six-storey apartment building, comprised of a total of 108 condominium units. It contributes to the overall supply of housing within the area and support the County's broader growth objectives.

## **B6 Population Forecast for 2034 by Municipality**

**Policy B6:** *Table G establishes the population forecasts for 2034 by municipality.*

The proposed development is located within the Urban Area of Port Hope, one of the County's six designated urban areas subject to the intensification targets. Table G indicates a population forecast of 22,145 for Port Hope. This represents an approximate 32 percent increase in population growth from 2011, and indicates a steady growth within the region.

**B7 Housing Forecast**

**Policy B7:** *Table H establishes the housing forecast for each of the local municipalities. These forecasts are considered to be guidelines that are to be considered by each local municipality when their Official Plans are amended to conform with this Plan. In this regard, local municipalities may adjust the housing forecast and in particular the housing mix as appropriate taking into account the nature of existing planning approvals and the policies of the local Official Plan, provided it can be demonstrated that conformity with Sections B9 and B10 can be achieved.*

**Policy B7 Analysis:**

Policy B7 of the NCOP, outlines the housing forecast for each of the local municipalities, including the Municipality of Port Hope. The total housing forecast for Port Hope is 2,436 residential dwelling units, which includes 924 medium density residential units.

The proposed development is located within the Urban Area of Port Hope, one of the County’s six designated urban areas subject to the intensification targets.

The proposal contemplates a six-storey apartment building, comprised of a total of 108 condominium units. It contributes to the overall supply of housing within the area and support the County’s broader growth objectives.

**B9 Minimum Intensification Target**

**Policy B9:** *The six urban areas in the County are required to accommodate a certain amount of projected population in the form of*

*intensification. The minimum intensification target for each of the six urban areas within the planning period is shown on Table I;*

*By the year 2015 and for each year thereafter, a minimum of 40 per cent of all residential development occurring annually within the County will be within the built boundary of the urban areas in accordance with Table I.*

*A review of the intensification targets established in Table I can be undertaken at the time of a County municipal comprehensive review.*

**Policy B9 Analysis:**

Policy B9 predicts each of the County’s six urban areas to accommodate projected population growth through residential intensification. This policy supports the County’s broader growth management direction, ensuring that settlement areas accommodate development efficiently.

The proposed development is located within the Urban Area of Port Hope, one of the County’s six designated urban areas subject to the intensification targets identified in Table I of the NCOP. It consists of a six-storey apartment building, comprised of a total of 108 condominium units and represents a significant form of residential intensification within the built-up area. The proposed development contributes to meeting the County of Northumberland and the Municipality of Port Hope’s intensification policies by:

- > Redeveloping underutilized and vacant lands within the existing settlement boundary;

- > Provide higher-density housing consistent with the NOP Urban Area, PHOP Residential - Medium Density and Major Intensification Area policies; and
- > Support the County's goal to accommodate population growth through compact, efficient built forms rather than outward expansion.

The NOP states that a minimum of 40% of all residential development occurring annually within the County will be within the built boundary of the urban areas. The proposed development will contribute to the County's target of 40% of residential development occurring within the urban area.

The proposal achieves 59 residents per gross hectare, which supports both the County and the Municipality intensification targets, and contributes to the required shared growth within the overall intensification framework.

### **B10 Minimum Greenfield Density Target**

**Policy B10:** *Within the six urban areas, the minimum density targets for new Greenfield development (residents and jobs combined per hectare) are set out on Table J. The County's minimum designated Greenfield area density target is 30 residents and jobs per hectare, combined for the urban areas in the County.*

#### **Policy B10 Analysis:**

Policy B10 requires that Greenfield development within each of the County's six urban areas achieve a minimum density of 30 residents and jobs per hectare. This policy supports the County's broader growth management

direction, ensuring that Greenfield areas occurs in a compact form within settlement areas to accommodate development efficiently.

> **Minimum required density:** 30 residents and jobs per hectare

> **Proposed density:** approximately 59 residents per hectare

The proposal achieves 59 residents per hectare, which exceeds the minimum Greenfield density requirements, and contributes to the required shared growth within the overall intensification framework.

## **Section C: Land Use Designations**

### **C1 Urban Areas / Rural Settlement Areas**

**Policy C1.1:** *The County encourages each of the six urban areas to become complete communities where there is:*

- a) *A strong live/work ratio, where the majority of residents are employed in the community in which they live;*
- d) *A range of housing types for all levels of income and ages;*
- g) *A range of cultural and recreational opportunities and facilities; and,*
- h) *A population level and density that supports the provision of public transit, where feasible.*

**Policy C1.1 (a,d,g,h) Analysis:**

The proposal contemplates a six-storey apartment building, comprised of a total of 108 condominium units. The proposal is for residential development, however, it contributes to the overall live/work ratio by increasing the population base within the Urban Area as per Schedule ‘A’ of NOP. An increase in local population nearby employment and commercial uses enables more residents to work within the community in which they live and support the County’s broader growth objectives.

The proposal contributes to the overall supply of housing within the area. It diversifies the housing typology and supports broader growth objectives by expanding options and reducing pressure on the existing housing stock.

The proposal contemplates a landscaped amenity space, providing opportunities for community programming and ‘third spaces’ through increased usage and participation.

The proposed development achieves a density of 59 residents per gross hectare, surpassing the County’s designated density targets. This level of density aligns with the County’s objective to concentrate growth in areas where transit-supportive development is most feasible.

**C1.2.1: It is the objective of this Plan to:**

- a) Maintain and enhance the character and identity of existing residential areas;*
- b) Encourage the provision of a range of housing types to accommodate persons with diverse social and economic backgrounds,*

*needs and desires while promoting the maintenance and improvement of existing housing;*

*c) Promote the efficient use of existing and planned infrastructure and public service facilities by supporting opportunities for various forms of residential intensification, where appropriate;*

*d) Encourage increases in density in new development areas to maximize the use of infrastructure and minimize the amount of land required for new development;*

*f) Encourage a high standard of urban design for development and redevelopment;*

*h) Implement street designs that provide for pedestrian, cycling and other non-motorized modes of transportation to help create more healthy and complete communities.*

**Policy C1.2.1 (a-d, f, h) Analysis:**

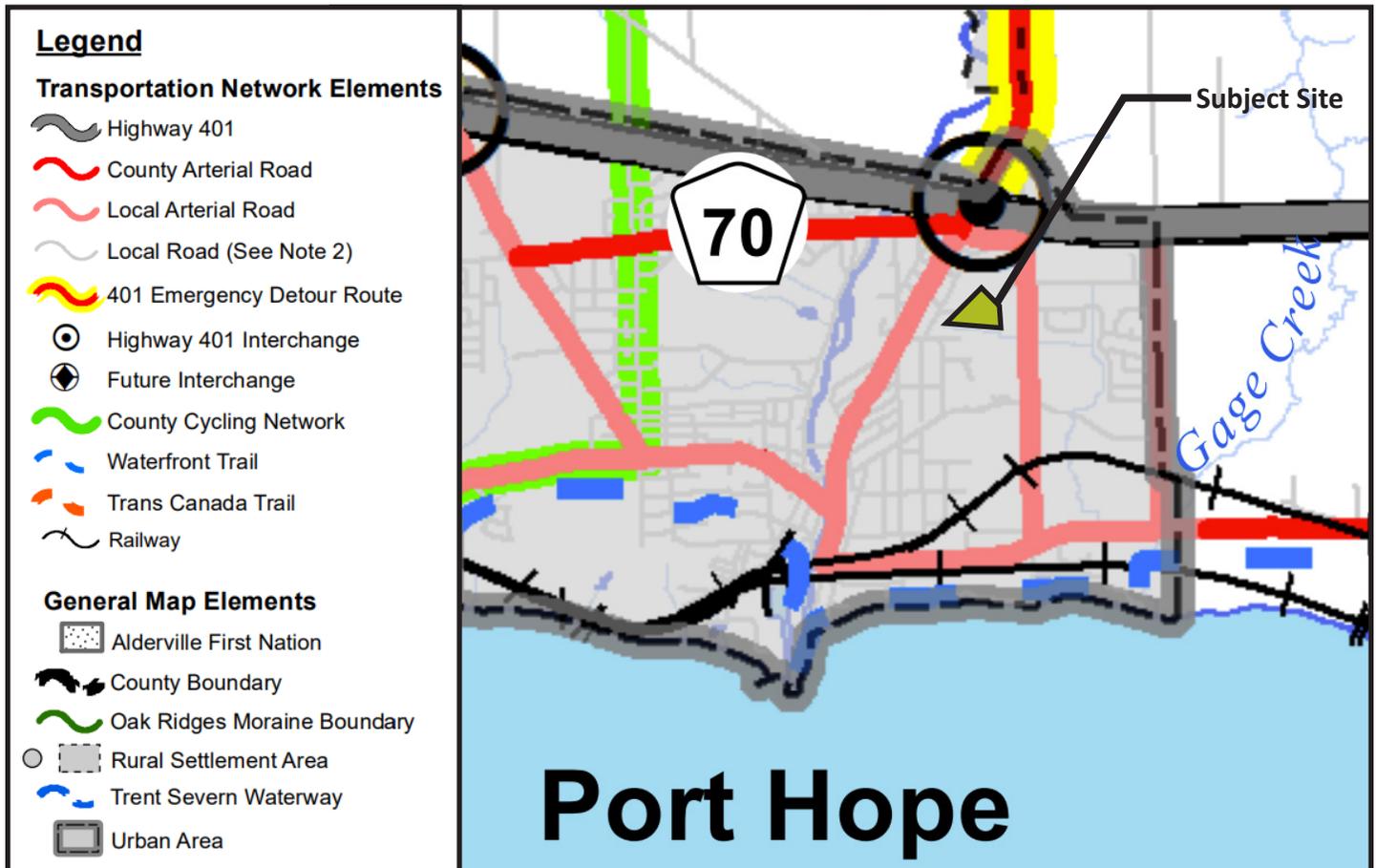
The site is located within the Urban Area on Schedule ‘A’ - Land Use of the County of Northumberland Official Plan (NOP) and Residential - Medium Density on Schedule ‘C1’ - Land Use of the Municipality of Port Hope Official Plan (PHOP). The proposed development contemplates a six-storey apartment buildings, comprised of a total of 108 condominium units and is located on a currently vacant parcel, allowing for a new built form to be introduced. The orientation toward the planned Croft Street extension ensure that the development integrates with the surrounding residential context and maintains the overall character of the neighbourhood structure.

The proposed development contributes to the overall supply of housing within the area. This medium-density residential development diversifies the housing typology and complements the 140-unit four-plex subdivision north of the site. This sectional massing transition creates distinct housing options suited to different household types, supporting a broader demographic range while maintaining compatibility.

The proposed development is supported by planned municipal infrastructure identified in the

Port Hope Asset Management Plan (2016), including the Croft Street Watermain/Sanitary Sewer Trunk Line. The site is located within the Urban Area of Port Hope, one of the County’s six designated urban areas subject to the intensification targets identified in Table I of the NCOP. It is also located in a Major Intensification Area on Schedule A-1 of the PHOP. The proposal aligns with the County and Port Hope’s intent for growth and efficiently utilizes planned servicing upgrades to support intensification.

Figure 1: Excerpt from Schedule ‘C’ - Transportation



The proposed density of 59 residents per hectare surpasses the County’s designated density objectives ensuring efficient land organization and maximizes the functional capacity of the planned collector road system and trunk infrastructure.

The proposed development incorporates a division of massing with the six-storey apartment building comprised of two (2) towers above a single-storey podium. The development incorporates massing with articulated facades, pedestrian-oriented entrances, landscaped amenity spaces and various exterior materials creating a unique design language and new threshold conditions.

The subject site fronts the planned Croft Street extension, identified as a Future Collector on Schedule D-1 (Transportation System) of the PHOP, which includes sidewalk and cycling infrastructure. The development’s internal circulation and sidewalk connections integrate directly into this planned network, improving walkability and linking residents to trails, parks, and neighbourhood destinations.

**Policy C1.5.1:** *It is the goal of this Plan to meet the County’s current and future housing needs by:*

- a) Monitoring and ensuring that there is a minimum 10 year supply of land for residential development in urban areas with sufficient water and wastewater capacity;*
- b) Ensuring the provision of an appropriate range of housing types and densities to meet the needs of current and future residents;*

*c) Assisting in the achievement of residential intensification and affordable housing by encouraging opportunities for mixed-use development in appropriate locations;*

*h) Supporting universal physical access and encourage the building industry to incorporate such features in new residential structures;*

*i) Encouraging the development and redevelopment of lands within settlement areas and in appropriate locations at higher densities to maximize the use of infrastructure;*

**Policy C1.5.1 (a-c, h, i) Analysis:**

The site is located within the Urban Area on Schedule ‘A’ - Land Use of NOP and Residential - Medium Density on Schedule ‘C1’ - Land Use of PHOP. The proposed development aligns with the extension of the Croft Street Watermain/ Sanitary Sewer Trunk Line identified in the Port Hope Asset Management Plan (2016), which enables the optimization of existing servicing systems and supports growth in an efficient, co-ordinated manner. The development contributes directly to the supply of designated and serviceable residential lands capable of supporting long-term growth forecasts.

The site is located in a Major Intensification Area on Schedule A-1 of the PHOP. The proposal aligns with the County and Port Hope’s intent for growth by proposing a density of 59 residents per hectare exceeding the County’s designated density objectives and providing housing to support the broader projected growth forecasts.

Although the proposal is strictly residential, it contributes to the intensification objectives by utilizing a vacant parcel within a designated Major Intensification Area (Schedule A-1). Increasing the amount of housing within the Urban Area contributes to the availability of more attainable units in the overall market.

The proposed development enhances accessibility for people of varying ages and abilities, and has regard for accessibility standards such as barrier-free entrances, elevators and parking. In addition, the proposed development will be designed and built in accordance with the Ontario Building Code and applicable Accessibility Requirements.

The proposed 108 dwelling units are serviced by a Future Collector on Schedule D-1 (Transportation System) of the PHOP, and the Croft Street Watermain/Sanitary Sewer Trunk Line, maximizing infrastructure efficiency. The density achieved helps concentrate growth within the Urban Area rather than extending settlement boundaries, aligning with the County's goal to direct development where servicing is available.

## **C6 Environmental Protection Area**

**Policy C6.1:** *It is the intent of this Plan to identify Provincially significant wetlands and Areas of Natural and Scientific Interest and to protect these features for the long term, in accordance with the policies of Section D1.5 of this Plan.*

### **Policy C6.1 Analysis:**

Schedule 'A' of the County Official Plan identifies Provincially Significant Wetlands (PSWs) and Areas of Natural and Scientific Interest (ANSI) for long-term protection.

The EIS prepared on August 12, 2025 by Cambium Inc., determined no PSWs or ANSIs occur on or adjacent to the subject site.

The two watercourse features on the site (north and south channels) were confirmed to be ephemeral, artificially created drainage channels with limited ecological function, lacking the characteristics of PSWs or provincially recognized natural heritage features.

Therefore, the proposed development does not encroach on, or create risk to any, PSWs or natural heritage features intended for long-term protection.

**Policy C6.2:** *The Environmental Protection Area designation is intended to identify and protect Provincially significant wetlands and Areas of Natural and Scientific Interest as classified by the Ministry of Natural Resources, and as identified on Schedule A to this Plan.*

### **Policy C6.2 Analysis:**

The Environmental Protection Area designation applies only to lands containing PSWs or ANSIs identified by the Ministry of Natural Resources and shown on Schedule 'A' of NOP. The site is not designated within the Environmental Protection Area, and no mapped natural heritage features under this designation intersect the site.

## **Section D: Resource Areas and Constraint Areas**

### **D2 Water Resources**

**Policy D2.1:** *The County and the local municipalities shall protect, improve or restore*

he quality and quantity of water by:

*b) Minimizing potential negative impacts, including cross-jurisdictional and cross-watershed impacts;*

*c) Identifying water resource systems consisting of ground water features, hydrologic functions, natural heritage features and areas, and surface water features including shoreline areas, which are necessary for the ecological and hydrological integrity of the watershed;*

*d) Maintaining linkages and related functions among ground water features, hydrologic functions, natural heritage features and areas, and surface water features including shoreline areas;*

*f) Planning for efficient and sustainable use of water resources, through practices for water conservation and sustaining water quality;*

*h) Ensuring stormwater management practices minimize stormwater volumes and contaminant loads, and maintain or increase the extent of vegetative and pervious surfaces.*

**Policy D2.1 Analysis:** The Stormwater Management (SWM) Report prepared by Jewell Engineering Inc., has considered any cumulative impacts of the proposed development within the applicable drainage area. The two watercourse features on the site (north and south channels) were confirmed to be ephemeral, artificially created drainage channels with limited

ecological function. The proposed development is not anticipated to create cross-jurisdictional or cross-watershed impacts. The site is clear of any hydrological functions, ground water, natural heritage and surface water features and will not have an impact on linkages or functions of these features.

The north and south channels, although disconnected and minimally functional, the proposed development maintains the existing drainage pathways and incorporates stormwater controls that preserve downstream hydrologic flow patterns.

The stormwater plan will incorporate low-impact development (LID) features where feasible, consistent with sustainable water use and conservation practices. This will minimize runoff volumes, reduces contaminant loads and maintains pervious surfaces.

**Policy D2.2: Restriction on Development and Site Alteration**

*a) Development and site alteration shall be restricted in or near sensitive surface water features and sensitive ground water features such that these features and their related hydrologic functions will be protected, improved or restored.*

**Policy D2.2 Analysis:**

Policy D2.2(a) requires restricting development near sensitive surface-water and groundwater features to ensure their hydrologic functions are protected, improved, or restored.

The EIS identifies two ephemeral drainage channels on the site that convey seasonal run-off but have limited hydrologic function. The apartment building is situated fronting Croft Street and therefore maintains a substantial setback from the drainage channels and the wetland on the southeastern corner of the site.

The proposed development maintains a buffer out of any sensitive areas and includes stormwater controls that maintain or enhance hydrologic function.

**Policy D2.4: Stormwater Management**

*a) Planning for stormwater management shall:*

*i) Minimize, or, where possible, prevent increases in contaminant loads;*

*ii) Minimize changes in water balance and erosion;*

*iii) Not increase risks to human health and safety and property damage;*

*iv) Maximize the extent and function of vegetative and pervious surfaces; and,*

*v) Promote stormwater management best practices, including stormwater attenuation and re-use, and low impact development.*

*b) In order to control flooding, ponding, erosion and sedimentation and to protect water quality and aquatic habitat or other natural habitat which depend on watercourses and other water bodies for their existence, stormwater management plans shall be required for any new*

*development consisting of more than four lots or for commercial or industrial developments with large amounts of impervious area. Stormwater management will be undertaken in accordance with Ministry of Environment and Climate Change (MOECC) Guideline "Stormwater Management Planning and Design Manual, 2003.*

*c) The County and the local municipalities shall require the use of stormwater management facilities downstream of new developments, where appropriate, to mitigate development impacts on stormwater quantity and quality.*

*d) The development of naturalized stormwater management facilities, constructed with gentle slopes is promoted, and should be designed in accordance with the Ministry of the Environment and Climate Change guidelines.*

**Policy D2.4 Analysis:**

The proposed development has incorporated measures that minimize contaminant loads, maintain the existing water balance, and reduce erosion through on-site controls. It ensures no increased risk to public safety or adjacent properties by keeping post-development flows at or below existing conditions.

The landscaped areas, tree planting, and pervious surfaces (approx. 67.5% of the site) maximize infiltration and reduce run-off. The design follows the MECP Stormwater Management Planning and Design Manual (2003), providing controls for flooding, erosion, sedimentation, and maintaining downstream water quality.

The EIS outlines these measures to ensure no adverse effects occur on

the two ephemeral channels on-site.

**D3.6 Cultural Heritage Resources**

**Policy D3.6:**

*a) The County recognizes that there are archaeological resources of precontact and early historic habitation as well as areas of archaeological potential within the County that can be adversely affected by any future development and redevelopment.*

**Policy D3.6 Analysis:**

Ironstone Archaeology Inc. has prepared a Stage 1 and Stage 2 Archaeological Assessments to determine archaeological potential of the site, primarily due to its proximity (within 300 m) to water resources and its potential to recover Indigenous resources. The Stage 1 Archaeological Assessment conducted on November 25, 2024 concludes the site retains Cultural Heritage Value or Interest (CHVI) and therefore requires a Stage 2 test-pit assessment. On May 6, 2025, a Stage 2 Archaeological Assessment was completed and no archaeological resources were found that require further assessment or mitigation. Therefore, archaeological concerns regarding have been appropriately addressed and conserved through the assessment process.

**Summary of Northumberland Official Plan Analysis**

It is our professional planning opinion that the proposed development conforms to the County of Northumberland Official Plan (NOP). The site is located within the Urban Area on Schedule 'A' - Land Use of the NOP, where growth, intensification, and higher-density residential uses are directed. The NOP identifies Urban Areas as the focus of

population and employment growth, supported by planned infrastructure such as the Croft Street Watermain and Sanitary Sewer Trunk Line.

The proposal contemplates a six-storey apartment building, comprising of a total of 108 condominium units and contributing to the overall supply of housing within the area. It represents residential intensification within a built boundary, and contributes to the County’s population and housing forecasts. It diversifies the housing options and supports broader growth objectives by expanding options on the existing housing stock, re-enforcing the broader intensification and growth strategy.

Environmental review confirms that no Provincially Significant Wetlands, ANSIs, or sensitive hydrologic features occur on or adjacent to the site. The ephemeral drainage channels and minor wetland feature have limited ecological function, and stormwater will be managed in accordance with provincial and municipal requirements. Archaeological assessments have been completed, with no further study required.

Overall, the proposed development is consist with the NOP, as it co-ordinates growth with efficient servicing, supports long-term sustainability, and effectively utilizes existing and planned infrastructure to meet current and future projected housing needs.

## 5.4 Municipality of Port Hope Official Plan (2008)

The Municipality of Port Hope Official Plan (PHOP) is a long-term local policy document that establishes a planning and land use framework on matters of public interest applicable within Port Hope, including guiding growth. The PHOP was adopted by Council on September 26, 2006 and approved with modifications by the Province in 2008. The Official Plan was updated (Official Plan Amendment No. 7) and approved with modifications in January 2017 by the Ontario Municipal Board.

The site is designated as follows under the PHOP and the various policies apply:

### Section C: General Development Policies

#### C1 Settlement Structure

*Schedule A - Settlement Structure geographically defines the areas referred to as the Urban Area, Hamlet, Rural Employment Area, Countryside Area and Oak Ridges Moraine.*

#### Settlement Area

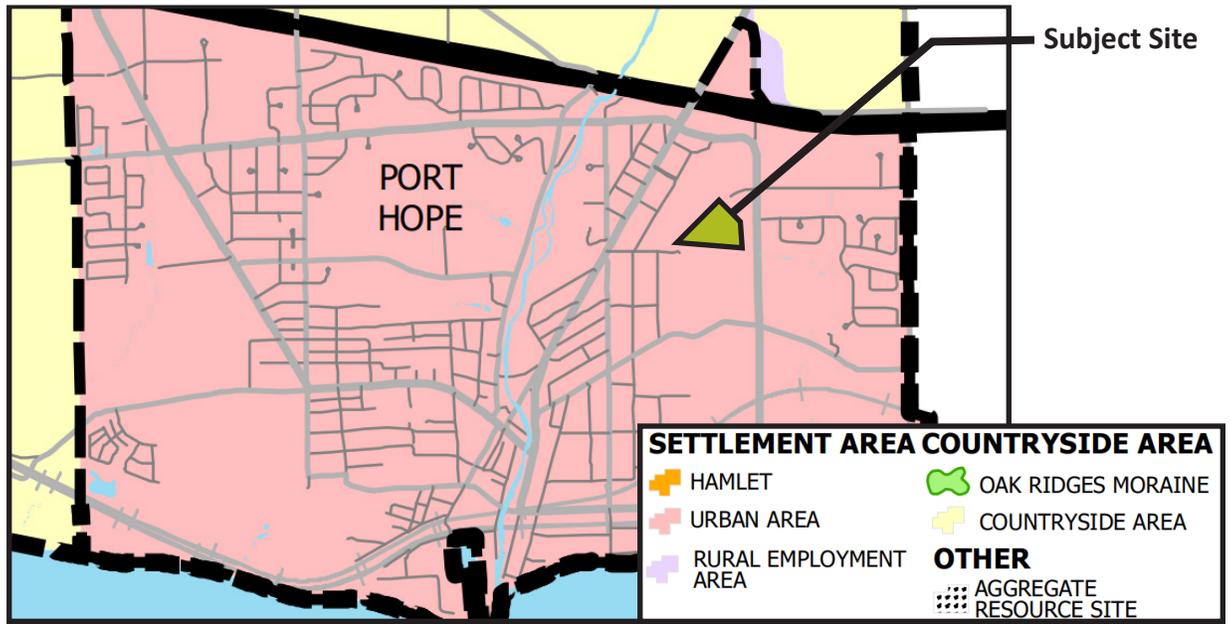
*a) Urban Area: The largest urban centre in the Municipality of Port Hope is planned to be the location of the widest range of commercial, employment-generating, and community facilities serviced by municipal water and sewer services;*

#### C1 a) Analysis:

The subject site is designated Residential - Medium Density on Schedule 'C1' - Land Use Urban Detail of the Municipality of Port Hope Official Plan (2017) and is within the Major Intensification Area' on Schedule 'A-1'. This allows for intensification and growth to be focused in areas where higher densities can be supportable with existing and/or planned services.

Municipality of Port Hope Official Plan Schedule	Designation
Schedule A - Settlement Area Countryside Area	Urban Area
Schedule A1 - Major Intensification Areas	Major Intensification Area
Schedule B-1 - Development Constraints Urban Area Detail	Wetland (Unevaluated) and Steep Slope
Schedule C-1 - Land Use Urban Area Detail	Medium Density Residential

**Figure 6:** Excerpt from Schedule 'A' - Settlement Area Countryside Area (PHOP)



**Figure 7:** Excerpt from Schedule 'A-1' - Major Intensification Areas (PHOP)

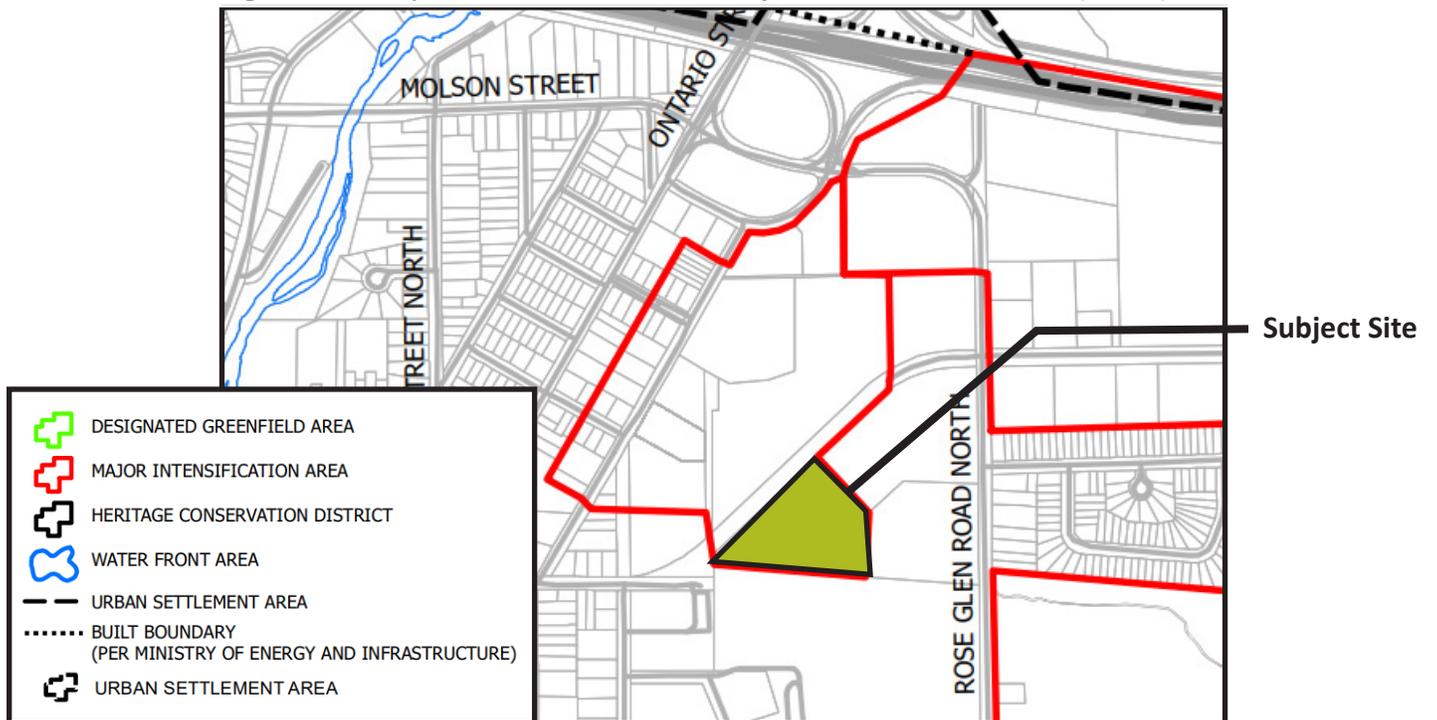
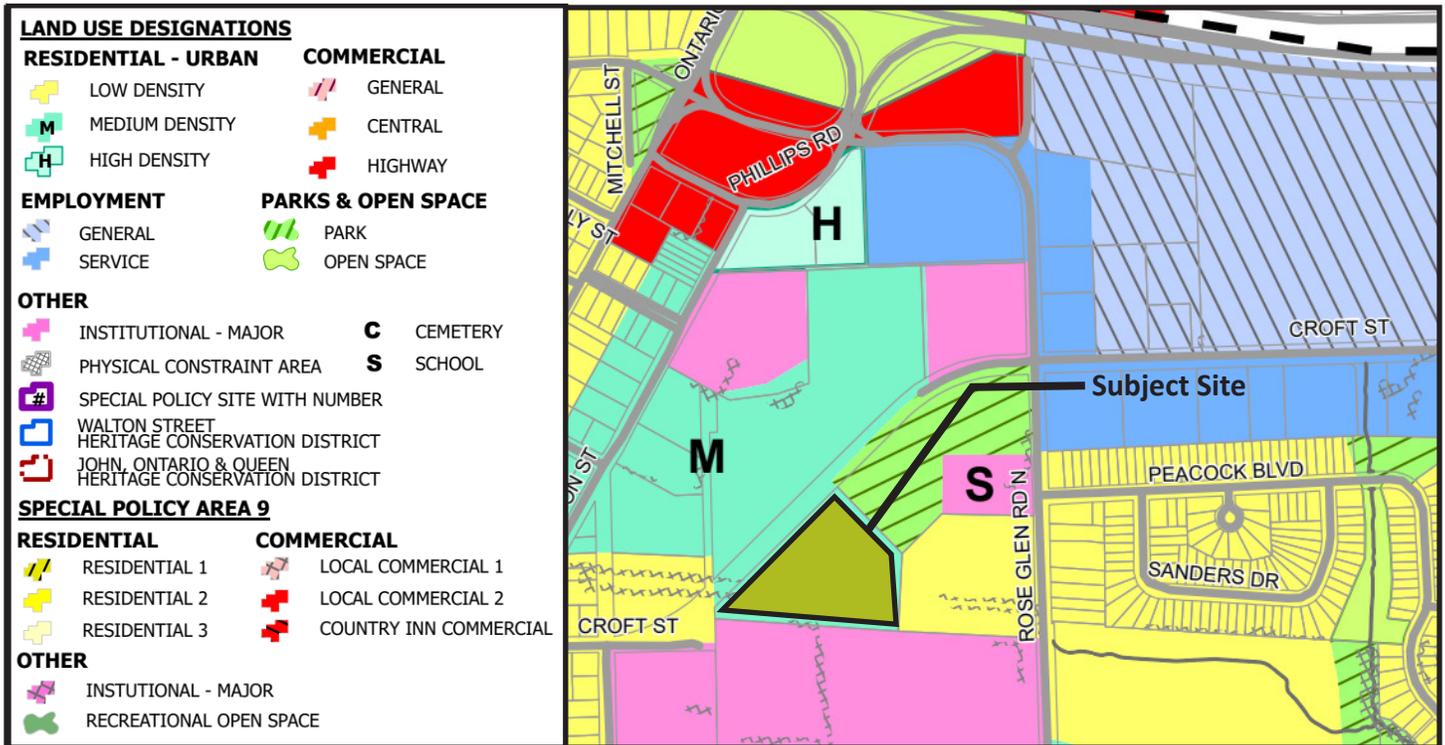


Figure 8: Excerpt from Schedule 'C-1' - Land Use Urban Detail (PHOP)



**C5 Environment**

**C5.1.2 Steep Slopes**

**Determination of Steep Slopes**

The extent and exact location of a Steep Slope shall be determined during consideration of development applications in accordance with detailed contour mapping or fill regulation and in consultation with the Ganaraska Region Conservation Authority. In the absence of such mapping or fill regulations, Council, in consultation with the Ganaraska Region Conservation Authority, shall use such Steep Slopes illustrated on Schedule B1 as general guidelines in the consideration of development applications in the Urban Area.

**Development Requirement**

Development within or adjacent to Steep Slopes is expected to conserve, where feasible, the natural elements, which assist in maintaining slope stability, soil conditions, and surface drainage. Accordingly, development shall be carried out in such a manner as to prevent slope instability, erosion, and disruption of the existing drainage patterns. All applications for development on lands of which any portion(s) includes Steep Slopes may be required to submit a steep slope analysis in keeping with the Implementation section of this Plan, to indicate how development may be appropriately designed and located. This requirement shall be met prior to any approval, or as a condition of any such approval.

**C5.1.2 Steep Slopes Analysis:**

The subject site is identified as containing areas of ‘Steep Slope’ and ‘Wetland (Unevaluated)’ on Schedule ‘B1’ – Development Constraints Urban Area Detail of the PHOP. These features are associated with the north and south watercourses traversing the site.

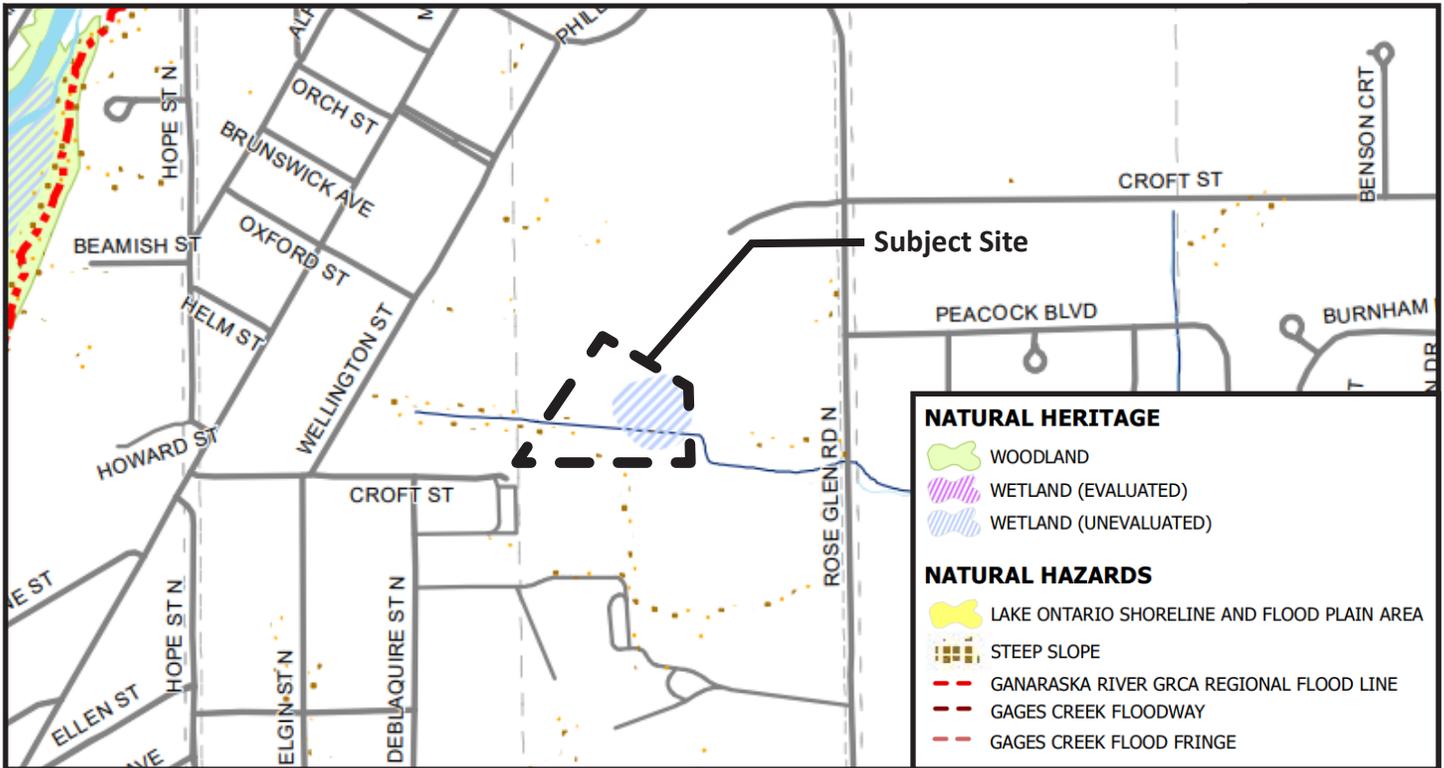
Steep Slopes are defined as slopes with horizontal to vertical gradients steeper than 3:1 and with a height greater than 3.0 meters. The proposed development will be reviewed in consultation with the Ganaraska Region Conservation Authority (GRCA) to confirm the presence, limits, and stability of the steep slope areas to demonstrate that slope stability, soil conditions, and existing drainage patterns will

be appropriately conserved and protected.

Any required mitigation measures will be implemented in accordance with GRCA standards and municipal requirements as a condition of development approval.

The proposed development generally conforms with the intent and requirements of Policy C5.1.2, subject to detailed technical review and approvals by the GRCA through the development application process.

**Figure 9:** Excerpt from Schedule ‘B-1’ - Development Constraints Urban Area Detail (PHOP)



## **C5 Natural Heritage Outside of the Oak Ridges Moraine**

### **C5.2.1 General**

*The Natural Heritage classification on schedules B and B1 identifies lands that are recognized by the Province, Ganaraska Region Conservation Authority and Council as containing one or more of the following significant or sensitive natural features or functions:*

*b) Wetlands - are lands that are seasonally or permanently covered by shallow water, as well as lands where the water table is close to or at the surface. In either case the presence of abundant water has caused the formation of hydric soils and has favoured the dominance of either hydrophytic plants or water tolerant plants. Wetlands that have been determined to be significant coastal wetlands as well as others that have not been evaluated have been specifically identified on Schedules B and B1. However, agricultural lands that are periodically soaked or wet and do not exhibit wetland characteristics are not considered to be wetlands for the purposes of this policy;*

### **C5.2.1 General Analysis:**

The EIS prepared on August 12, 2025 by Cambium Inc., determined the two (2) watercourse features on the site (north and south channels) were confirmed to be ephemeral, artificially created drainage channels. These channels consist of limited ecological function, lacking the characteristics of PSWs or provincially recognized natural heritage features.

Therefore, the proposed development does not encroach on or create risk to any PSWs or

natural heritage features intended for long-term protection.

### **C9.1.2.3 Housing Intensification**

*The intensification of residential development reduces the need to use vacant designated land on the periphery of the Urban Area. It also reduces the need for urban expansions into the rural area. Residential intensification of existing areas allows for the efficient provision of urban services thereby helping to minimize the costs of providing services while meeting an important component of the Municipality's housing needs.*

*The following shall be the policy of Council:*

*a) Housing shall, in part, be provided through residential intensification, which shall include but not be limited to any of the following:*

*ii) infilling development and residential development of vacant land or underutilized land in existing neighbourhoods;*

*b) The Municipality shall consider applications for infill development, intensification and redevelopment of sites and buildings through intensification based on the following criteria:*

*i) the development proposal is within the Urban Area;*

*iii) the road network can accommodate the traffic generated;*

*iv) the height and density of development is appropriate and compatible with the scale of*

*adjacent development, and the character of the neighbourhood; and,*

*v) the development proposal will not have a negative impact on cultural heritage resources or natural heritage features in the area.*

#### **C9.1.2.3 Housing Intensification Analysis:**

The proposed development represents an appropriate and planned form of residential intensification within the Urban Area of Schedule 'A' of the PHOP. The subject site is designated Residential - Medium Density on Schedule 'C1' - Land Use Urban Detail of the PHOP and is currently vacant, underutilized land. It is projected that the population in the Urban Area will increase by 4,531 people by 2034. Growth is to occur within existing settlement areas where existing and future planned services are available and where a wide range of uses are accessible to foster a complete community. The site is also located in a Major Intensification Area (Schedule A1), which recognizes the site to be supportive of higher density development. The proposal contemplates a six-storey residential apartment building consisting of a total of 108 condominium dwelling units and a net density of 59 units/hectare. This proposal represents appropriate infill development and redevelopment of vacant land.

The project supports the efficient use of existing and planned municipal infrastructure of the Croft Street extension, which further limits the need for urban sprawl, and contributes directly to the local housing supply and intensification objectives.

A Transportation Impact Study (TIS) prepared for the proposed development demonstrates that the surrounding road network, including Rose Glen Road North (arterial roadway), and the proposed Croft Street extension, can accommodate the additional traffic generated by the 108-unit residential development. The TIS confirms that intersection operations and roadway capacity will continue to function within acceptable levels of service following implementation of the development.

The scale and massing of the buildings are compatible with the evolving residential character of the surrounding area and provide an appropriate transition to adjacent low-density residential and naturalized areas through building setbacks.

An Environmental Impact Study (EIS) prepared in support of the applications evaluates the natural heritage features associated with the subject site, including areas identified on Schedule 'B1' – Development Constraints Urban Area Detail, such as Steep Slope and Wetland (Unevaluated) features related to the north and south watercourses. The EIS concludes that, with implementation of recommended mitigation measures and continued review by the Ganaraska Region Conservation Authority (GRCA), the proposed development will not result in negative impacts to natural heritage features or ecological functions. No adverse impacts to cultural heritage resources have been identified.

#### **C11.3.2 Design for People**

*Council shall promote the design of buildings and spaces to be functional for people of all ages.*

*Council shall encourage buildings and spaces that establish a pedestrian scale by promoting:*

*a) the placement of continuous horizontal features on the first two storeys adjacent to the road;*

*b) the repetition of landscaping elements, such as trees, shrubs or paving modules; and*

*c) the use of familiar sized architectural elements such as doorways and windows.*

#### **C11.3.2 Design for People Analysis:**

The ground-oriented portions of the proposed six-storey residential apartment building incorporate continuous horizontal architectural elements along the first two storeys, including material treatments which visually enhance the pedestrian experience.

The landscape design incorporates the repetition of street trees, low shrubs, and co-ordinated paving materials along pedestrian walkways and building entrances. These elements re-inforce visual continuity along the street and improve walkability and site legibility.

The building façades utilize familiar-sized architectural elements, including clearly defined residential entrances, regularly spaced windows, and transparent ground-floor glazing to provide scale, and visual interest at the pedestrian level.

#### **C11.3.3(b)(c)(f) Design with Nature**

*Council shall encourage a proposed development or infrastructure undertaking in or adjacent to a Natural Heritage feature to maintain the*

*integrity of the area through designs that:*

*b) conserve the largest area of significant natural features and functions as possible;*

*c) limit the amount of natural edge that is exposed to development;*

*f) provide for the protection and restoration of the area.*

#### **C11.3.3(b)(c)(f) Design with Nature Analysis:**

The proposed development is designed to maintain the integrity of on-site and adjacent natural heritage features in accordance with the PHOP. The subject site contains natural features identified on Schedule 'B1' - Development Constraints Urban Area Detail, including areas associated with steep slopes and unevaluated wetlands related to the north and south watercourses. An Environmental Impact Study (EIS) prepared in support of the application guides the proposed development to ensure the integrity of natural features is maintained.

The site layout conserves the largest area of significant natural features to the south and southeast with the building footprint located to the north to front along the proposed Croft Street extension strategically located away from sensitive areas.

The natural edge exposed to development is limited through compact built form, access points located to the north and the clustering of built form toward the central portion of the site, thereby reducing encroachment into the naturalized areas.

The proposed development includes measures for the long-term protection and restoration of natural features, including native species planting, erosion and sediment control measures during construction, and post-construction measures to ensure the health of potentially disturbed buffer areas, as recommended in the EIS.

**C11.3.4(a)(b)(c) Design for Energy Conservation**

*Consistent with federal and provincial policies, Council shall encourage energy conservation by approving developments that:*

- a) incorporate energy efficient arrangements, such as through the orientation of buildings and the capacity to provide alternative energy supplies;*
- b) provides for pedestrian and bike paths and facilities;*
- c) have a compact pattern of development that clusters compatible uses within close proximity to one another;*

**C11.3.4(a)(b)(c) Design for Energy Conservation Analysis:**

The proposed apartment building is oriented to maximize exposure to natural daylight and allow for passive solar gain where feasible, thereby supporting reduced energy consumption for lighting and heating.

The site design provides for safe and direct pedestrian connections between building entrances, parking areas, and the Croft Street

extension sidewalk. The development is located in proximity to existing and planned pedestrian and active transportation routes along Rose Glen Road North and Croft Street supporting walkability and cycling as alternatives to vehicle trips.

The proposed development represents a compact form of residential intensification, with a six-storey apartment building. It is comprised of two (2) towers connected via a single storey podium and supported by a one (1) storey underground parking garage. This compact development pattern coupled with higher density reduces infrastructure demands, and supports long-term energy efficiency and sustainability.

**C11.3.5(a-c) Integration of Built Form [Built Form]**

*Council shall ensure that the design of new development:*

- a) is complementary to adjacent development in terms of its overall massing, orientation and setback;*
- b) provides links with pedestrian, cycling and road networks;*
- c) extends the existing road pattern and character to enhance orientation and integrate newly developing areas of the Municipality of Port Hope;*

### **C11.3.5(a-c) Integration of Built Form Analysis [Built Form]**

The scale, massing, orientation, and setbacks of the proposed apartment building is complementary to the surrounding low- and medium-density residential uses. Buildings are oriented toward Croft Street and set back appropriately from adjacent property lines to provide suitable transitions in height and maintain a consistent streetscape character.

The site design incorporates direct connections to the existing and planned pedestrian, cycling, and road networks, including sidewalks along Croft Street and connectivity to Rose Glen Road North, where Port Hope Transit Route B is located. Internal paved walkways provide safe and convenient access between building entrances, parking areas, and the outdoor amenity spaces.

The proposal extends and re-inforces the existing road pattern through the planned extension of Croft Street, including the proposed cul-de-sac. This road extension facilitates the integration of the subject site into the surrounding municipal road network.

## **Section C: General Development Policies**

### **C12 Infrastructure**

#### **C12.1.1.1(a-c) Water Supply**

*Water supply facilities shall be designed, constructed and maintained to:*

*a) provide adequate service to the proposed development;*

*b) accommodate full development of the service area; and*

*c) satisfy the servicing standards of the Municipality of Port Hope.*

#### **C12.1.1.1(a-c) Water Supply Analysis**

The Functional Servicing Report (FSR) prepared by Jewell Engineering Inc. (Jewell), confirms that municipal services can support the proposed six-storey residential apartment building and the associated 108 residential units. Measures will be implemented to ensure water servicing will be designed to meet both domestic demand and fire flow safety requirements.

The proposed water supply connections are intended to accommodate the full range of development throughout the service area, including future planned growth within the Croft Street corridor. The proposal includes a 200mm watermain along Croft Street and a 150mm service to the development site as part of the proposed Croft Street extension. This will facilitate co-ordinated servicing with surrounding lands as they develop, where currently are to be single metered with a shutoff valve at the property line.

All water supply services will be designed, constructed, and maintained in full compliance with the servicing standards of the Municipality of Port Hope, subject to detailed review and approval through Site Plan Approval and the Building Permit process.

**C12.1.3 Storm Water Management**

**Appropriate Facilities**

*Developments shall provide appropriate storm water management facilities in accordance with current Ministry of the Environment and Climate Change Guidelines as approved by the Municipality, the Ganaraska Region Conservation Authority and, where necessary, the Ministry of Transportation.*

**C12.1.3 Storm Water Management Analysis**

The proposed development is supported by appropriate stormwater management in accordance with the Ministry of the Environment

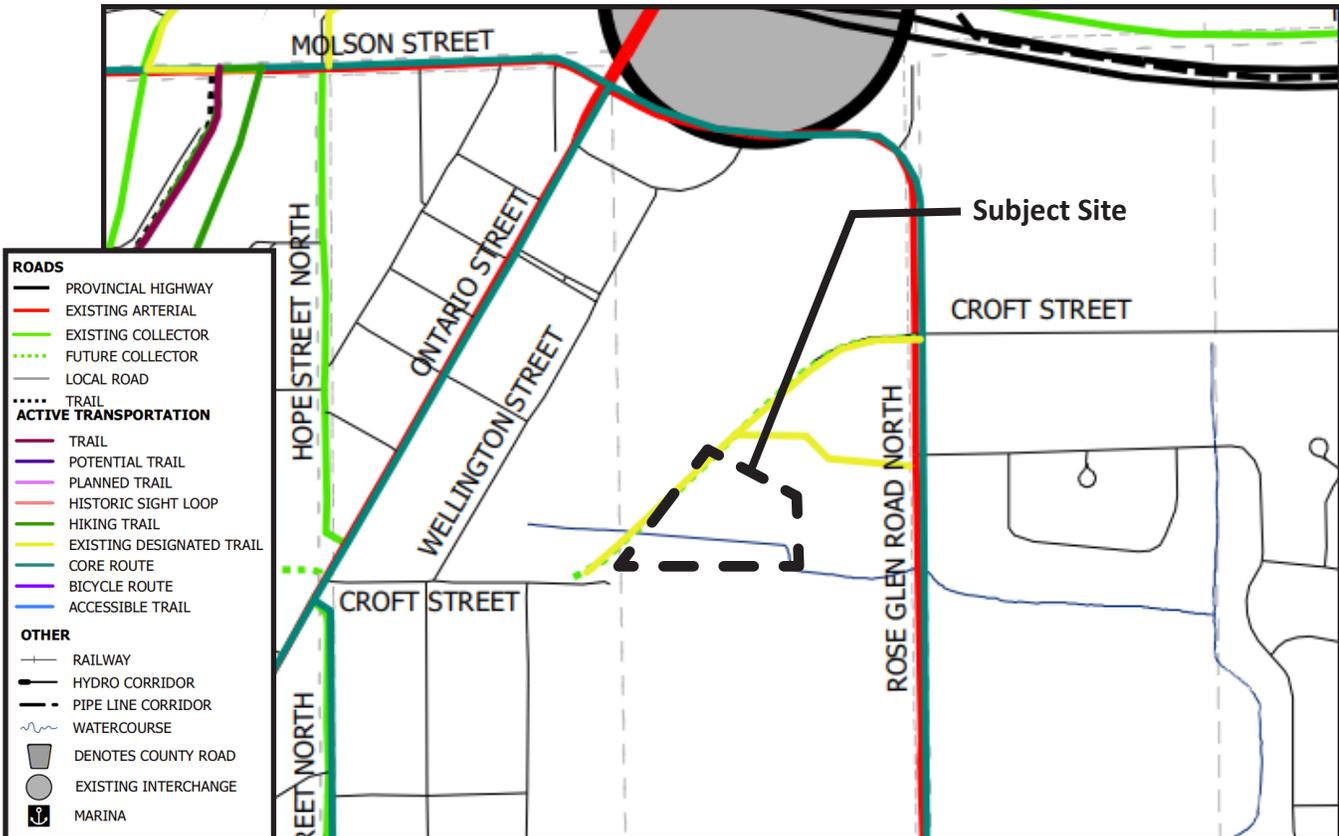
and Climate Change (MECP) Guidelines, as outlined in the Functional Servicing and Stormwater Management Report.

**C13.2.4 Collector Roads**

**Location Schedule D1**

*Collector roads, as identified on Schedule D1 are existing and proposed roads with a minimum of 2 traffic lanes and a right-of-way width of 23 to 26 metres, which are designed to collect and carry local traffic to, and from the arterial roads and to provide access to land.*

**Figure 2:** Excerpt from Schedule ‘D-1’ - Transportation System Urban Area Detail of the PHOP



**Access Limitations**

*Access points onto collector roads shall be controlled so as not to interfere with their function of providing for through traffic movements.*

**Sidewalks**

*Sidewalks shall be provided on both sides of collector roads in the Urban Area.*

**C13.2.4 Collector Roads Analysis:**

The proposed development is supported by the planned Croft Street Extension, which functions as both Future Collector Road and Local Road within the Urban Area. It is intended to provide connectivity between Rose Glen Road North and the broader road network as identified on Schedule 'D1' - Transportation of the PHOP. The roadway is designed to accommodate a minimum of two (2) traffic lanes and an appropriate right-of-way (ROW) width to safely collect and convey traffic while providing access to abutting lands.

Vehicular access to the subject site is provided through three (3) access points along Croft Street (eastern, central, and western). The Traffic Impact Study (TIS) prepared by Jewell Engineering Inc. (Jewell), confirms the Croft Street and Rose Glen North intersection provided good levels of service in both AM, PM and Saturday Peak times. This approach limits interruptions to through traffic. Further detailed findings are available in the Traffic Impact Study.

The proposed development incorporates sidewalks on both sides of the Croft Street Extension. These sidewalks will provide safe, accessible pedestrian connections between the subject site and the surrounding road, transit, and active transportation network, including

Route B Transit Line on Rose Glen Road North.

Overall, the proposed development conforms with the intent and requirements of Schedule D1 by ensuring appropriate roadway function, controlled access, and the provision of pedestrian infrastructure.

**C13.6 Parking****On-street**

*On-street parking facilities shall only be permitted where they do not interfere with traffic flows or create traffic hazards.*

**Buffering**

*Parking areas shall provide adequate buffering to adjacent land uses*

**C13.6 Parking Analysis:**

The proposed development consists of at-grade parking consists of 26 parking spaces including two (2) barrier-free spaces. Below-grade (one-storey) parking consists of 132 parking spaces, including four (4) barrier-free spaces.

Parking areas within the development site will incorporate buffering along Croft Street, with soft landscaping, setbacks and screening to soften the visual impact from the public realm.

**C13.7 Active Transportation****Safe, Accessible, and Secure**

*In all new development and redevelopment, provision shall be made for safe, accessible and secure pedestrian and cycling movements.*

**Walkways**

*Pedestrian walkways shall be provided to connect sidewalks along road rights-of-way and bicycle ways, where appropriate.*

**C13.7 Active Transportation Analysis:**

The proposed development will provide continuous, accessible pedestrian walkways on both sides of the Croft Street Extension, which is identified as a Future Collector with sidewalk and cycling facilities on Schedule D-1 of the PHOP. These walkways will create direct, barrier-free connections between the site and surrounding neighbourhood amenities, and will ensure integration with future cycling routes where appropriate.

**Section D: Land Use**

**D2 Residential**

**D2.1.3 Medium Density Residential**

**Unit Types**

*Within Residential - Medium Density as designated on Schedule C1, the main permitted uses shall include townhouse, rowhouse, fourplex, low-rise apartments, and other forms of low-rise multiple dwellings.*

**Maximum Density**

*The net residential density for residential development shall generally be between 20 and 60 units per net residential hectare.*

**D2.1.3 Medium Density Residential Analysis:**

The site is designated as Residential - Medium Density on Schedule 'C1' - Land Use and Major Intensification Area on Schedule 'A-1' of the Municipality of Port Hope Official Plan (PHOP). The proposed development has a net density of 59 units per hectare, which falls within the Medium Density range of 20–60 units per hectare. The proposal meets the upper limit of the designation and satisfies the general intent to support compact residential forms in intensification areas.

However, the Medium Density designation identifies permitted built forms such as townhouses, rowhouses, fourplexes, and low-rise apartments. The proposed built form consists of a six-storey residential apartment building consisting of two (2) towers connected via a central common lobby corridor, which exceeds what is typically understood as 'low-rise' and therefore does not fully align with the built form of the Medium Density designation. While Council may consider additional height or density within Major Intensification Areas, the building height and massing exceed the typical range contemplated for Medium Density Residential areas.

Appropriate buffering measures (setbacks, landscaping, screening) can be incorporated to ensure compatibility with adjacent low- and medium-density areas, however, the scale of development more closely reflects the Residential - High Density designation (Schedule 'C1' of the PHOP).

#### **D2.1.4 High Density Residential**

##### **Unit Types**

*Within Residential - High Density as designated on Schedule C1, the main permitted uses shall include apartments and other forms of multiple dwellings.*

##### **Maximum Density**

*The net residential density for the high density residential developments shall generally be between 40 and 140 units per net residential hectare.*

##### **Comprehensive Development Plans**

*Lands designated Residential - High Density shall be developed on the basis of comprehensive development plans which are intended to indicate the layout of buildings, parking and landscaping areas, and access points over all lands within the designation, such that development on individual properties may be integrated with that of adjacent properties, with particular regard for common elements such as access points, driveways, landscaping, and parking areas.*

##### **Buffer from Low Density Residential**

*Where development within a Residential - High Density designation is in proximity to development within a Residential Area - Low or Medium Density designation, potential adverse impacts between the developments shall be mitigated through building setbacks, visual screening, landscaping, fencing, and other forms of buffering, in accordance with the provisions of this Plan.*

#### **D2.1.4 High Density Residential Analysis:**

The Residential - High Density designation specifically permits apartments and multiple dwellings, which aligns better with the proposed six-storey built form. The proposed development has a net density of 59 units per hectare, which falls within the High Density range of 40–140 units per hectare, placing it comfortably within this policy framework. It satisfies the general intent to support compact residential forms in intensification areas.

The proposed development is planning comprehensively, with respect to ensure growth is supported by planned infrastructure. The planned Croft Street extension and trunk servicing upgrades further support this form of higher-density urban development.

Given the building height, massing, unit count, and the site located within a Major Intensification Area, the proposed development is more appropriately aligned with the High Density Residential designation, both in permitted built form and in intended urban function. The High Density policies provide stronger and more explicit support for this scale of intensification within the Port Hope Urban Area.

### Summary of Port Hope Official Plan Analysis

The proposed development generally conforms to the policies of the Port Hope Official Plan (PHOP), specifically with respect to those related to intensification, growth, housing and complete communities. The built form and land use proposed for the site fits well within the existing neighbourhood context and provides appropriate transition to nearby low-density and high-density residential uses. The proposed height and density on the subject site is compatible with the adjacent land uses. The development positively contributes to the streetscape on Croft Street through appropriate building design and landscaping.

Overall, the proposed development generally conforms with the PHOP, as it co-ordinates growth with efficient servicing, supports long-term sustainability, and effectively utilizes existing and planned infrastructure to meet current and future projected housing needs.

With the Official Plan Amendment to change the subject site from Residential - Medium Density to Residential - High Density to permit apartment dwellings, it allows for an increase in the diversity of housing stock in Port Hope, and promotes the creation of a complete, walkable, communities.

#### 5.4.1 Official Plan Amendment

The site is designated as Residential - Medium Density on Schedule 'C1' - Land Use in the PHOP, which supports the proposed land use. However, the proposed built form consists of a six-storey apartment building which exceeds what is typically understood as 'low-rise' and therefore does not fully align with the built form of the Medium Density

designation.

A site-specific Official Plan Amendment is required to permit the apartment dwelling uses as permitted by Policy D.2.1.4 [High Density Residential], and Schedule 'C1' - Land Use in the PHOP. The Draft Official Plan Amendment is attached as Appendix X. The following amendments to the Official Plan are proposed for the subject site:

**Land Use:** Amend the subject site on Schedule 'C1' - Land Use from Residential - Medium Density to Residential - High Density to permit apartment dwellings, which aligns with the proposed development.

## 5.5 Municipality of Port Hope Zoning By-law 20/2010 (2011)

The Municipality of Port Hope Zoning By-law 20/2010 (PHZBL) was adopted on June 29, 2010, with OMB Approval on February 14, 2011. Amendments to the By-law include By-law 31/2023 which is the current updated version. The purpose of the Zoning By-law is to regulate the use of land, buildings and structures to implement the Port Hope Official Plan (PHOP).

The subject site is currently zoned as Low Density Residential One - Type One Holding Symbol 1 'RES1-1(H1)' within Schedule 'A-9' of the Municipality of Port Hope Zoning By-law 2020/10, as amended by By-law 31/2023 (PHZLB). The Holding Symbol-One (H1) provision applies to the subject site. As per Table 13.1 (Holding (H) Provisions) of the Zoning By-law, the Holding (H1) provision may be lifted once the Municipality is satisfied that an appropriate Subdivision Agreement and/or Site Plan Agreement and/or Development Agreement has been executed.

The Low Density Residential One 'RES1-1' Zone permits a range of residential uses. However, Apartment Dwellings are not permitted within this zone. With this, the subject site will require an amendment from the Low Density Residential One 'RES1-1' Zone to the new High Density Residential Exception XX Zone (RES4-XX), which permit Apartment Dwellings.

Areas of non-compliance to the Zoning By-law will be addressed through a site-specific exception zone and are included in the Draft Zoning By-law Amendment (refer to Appendix X). This Draft By-law would create site-specific exceptions in the Zoning By-law 20/2010.

The following outlines the requested amendments to the Zoning By-law to accommodate the proposed development.

The requested High Density Residential Exception XX Zone (RES4-XX) provisions are as follows:

### Permitted Uses

- All uses permitted under the High Density Residential Zone (RES4) as per Table 6.1 of the Zoning By-law;
- To permit, three (3) driveways on lots that have a lot frontage of 22.0 metres or greater;
- To permit loading spaces to be located in the front yard;
- To permit a maximum width of a driveway at the streetline of 8 metres;
- Where there is an opaque wall or fence having a height of 1.5 metres or more, the width of the planting strip is permitted to be reduced to 1.8 metres in width.

All other provisions and requirements of the RES4 Zone will apply to the proposed RES4-XX Zone.

Table 3: Standards for High Density Residential 'RES4' Zone		
Provision	Required	Proposed
Minimum Lot Area	1 hectare	Approx. 1.85 hectares
Minimum Lot Frontage	60 metres	Approx. 140 metres
Minimum Required Front Yard (1)	10.16 metres	17.2 metres
Minimum Required Exterior Side Yard (1)	10.16 metres	20.3 metres
Minimum Required Interior Side Yard (1)	10.16 metres	19.1 metres
Minimum Required Rear Yard (1)	10.16 metres	11.8 metres
Maximum Height	N/A	23 metres (peak of roof)
<p><b>Special Provisions</b>                      (1) Plus an additional 1.0 metre for every 3.0 metres or part thereof by which the apartment building exceeds a height of 15.0 metres accounted for.</p> <p><i>Note: the above setback requirements are calculated using the proposed height of 23 metres. Therefore, 2.66 metres has been added onto the minimum required setbacks in accordance with Special Provision (1).</i></p>		

The site specific amendments are highlighted in Table 4, below.

Table 4: Applicable Zoning Provisions		
Provision	Required	Proposed
<b>4.25 Planting Strips</b>		
4.25.1.b).i)	A 4.5 metre wide planting strip abutting the full length of the lot line shall be required along a streetline where a lot contains an apartment building.	2.1 metres
4.25.1.d)	Where there is an opaque wall or fence having a height of 1.5 metres or more, the width of the planting strip is permitted to be reduced to 1.8 metres in width.	2.1 metres
<b>5.2 General Parking Provisions</b>		
5.2.3.1.b)	Where parking spaces are provided in an enclosed or underground parking garage, such parking spaces shall have a width of not less than 2.7 metres and a length of not less than 5.4 metres.	2.74 by 5.8 metres
5.2.8.2	The minimum width of an aisle providing access to a parking space within a parking area shall be 3.5 metres, except as set out in Table 5.1, in the case of angled off-street parking accessed by a one-way aisle.	6.7 metres

Provision	Required	Proposed
<b>5.5 General Residential Parking Provisions</b>		
5.5.1.d)	A parking area associated with an apartment or multiple-unit building shall be set back 7.5 metres from a streetline, 3.0 metres from an interior side lot line and 1.8 metres from the building on the same lot.	Provided
<b>5.5.3 Driveways in Residential Zones</b>		
5.5.3.1.a)	The minimum driveway width shall be 3.0 metres.	5.8 metres
5.5.3.1.b)	The maximum driveway width shall be equal to the garage door width plus 1.5 metres or 6.1 metres, whichever is greater, provided a minimum of 40% of the area of the front exterior side yard in which the driveway is located is the site of soft landscaping.	8 metres
5.5.3.1.c)	Notwithstanding Section 5.5.3.1(b) above, the maximum width of a driveway at the streetline shall be 6.1 metres.	8 metres
5.5.3.3	On lots that have a lot frontage of 22.0 metres or greater, a second driveway crossing the same lot line as the first driveway is permitted provided:	Three (3) driveways
5.5.3.3.a)	The principal building is setback at least 15.0 metres from the streetline;	17.2 metres
5.5.3.3.b)	The driveways are at least 7.0 metres apart, measured at the streetline;	Greater than 7.0 metres
5.5.3.3.c)	The second driveway allows a motor vehicle alternate access to a lot, and connects from the streetline to the main driveway providing access to the private garage, carport or other parking spaces; and,	Yes
5.5.3.3.d)	No more than 50% of the area of the front yard and 30% of the width of the lot frontage is used for driveway purposes.	50 % and 10.3 %
<b>5.7 Loading</b>		
5.7.1	Each loading space shall be a minimum of 3.7 metres wide, 9.0 metres long and have a minimum vertical clearance of 4.3 metres.	Provided
5.7.2.a)	Be located on the same lot for the use or building for which it is required;	Yes
5.7.2.c)	Not be permitted in the front yard or exterior side yard, and shall not be located in any required yard;	No, loading spaces are located in the front yard
5.7.2.d)	In a Residential Zone, be set back a minimum of 10.0 metres from any streetline and 3.0 metres from an interior side or rear lot line;	17.2 metres

The following are the applicable parking rates requires as per Table 5.3 (Non-Residential Parking Requirements) and Table 5.5 (Residential Parking Requirements).

Table 5: Applicable Parking Rates for Non-Residential and Residential Uses			
	Parking Rates	Required	Proposed
Dwelling, Apartment	1 per unit, plus 0.25 per unit dedicated for visitor parking	<ul style="list-style-type: none"> <li>• 108 parking spaces</li> <li>• 27 visitor parking space</li> </ul>	<ul style="list-style-type: none"> <li>• 142 parking spaces</li> <li>• No visitor parking spaces specified</li> </ul>
Restaurant (1)	1 per 9.3 m <sup>2</sup> of net floor area	12 parking spaces	12 parking spaces
<b>Total</b>		<b>147 parking spaces</b>	<b>154 parking spaces</b>
<i>(1) Parking requirements may increase if a patio accessory to the restaurant use is proposed. See Section 5.2.7 (Parking Required for Outdoor Patios).</i>			

### 5.5.1 Rezoning Rationale

The proposed development is not permitted under the Port Hope Zoning By-law 20/2010 (PHZBL), as amended by By-law 31/2023. The Zoning By-law Amendment is required to rezone the subject site from the current Low Density Residential One - Type One Holding Symbol 1 'RES1-1(H1)' Zone to the new High Density Residential Exception XX Zone (RES4-XX). This will allow the subject site to permit Apartment Dwellings, with exceptions related to the landscape strip, loading spaces, and driveways.

The proposed Zoning By-law Amendment promotes an efficient use of land on a relatively large lot, is well located within an established neighbourhood and can accommodate significant growth. The Amendment supports the intensification of an underutilized site that has planned municipal infrastructure through the Croft Street extension.

With the Zoning By-law Amendment to change the subject site from Low Density Residential One - Type One Holding Symbol 1 'RES1-1(H1)' Zone to the new High Density Residential Exception XX Zone (RES4-XX), to permit apartment dwellings, it allows for an increase in the diversity of housing stock in Port Hope, and promotes the creation of a complete, walkable, communities.

## 6.0 PLANNING ANALYSIS

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Based on the policy analysis undertaken, it is our professional planning opinion that the proposal is consistent with the Provincial Planning Statement, conforms to the County of Northumberland Official Plan and Municipality of Port Hope Official Plan policies. The following sub-sections highlight key planning merits of the proposal and outline how the proposed development represents good planning.

### 6.1 Desirable Growth

Growth and redevelopment is appropriate for the subject site given its location on the south side of the Croft Street extension, east of Wellington Street, and between Deblaquire Street North and Rose Glen Road North, within the Town of Port Hope urban boundary where the PPS directs growth to occur. Given the large irregular site size of approximately 18,580 m<sup>2</sup> (4.6 acres), and its proximity to Croft Street and Rose Glen Road North, the proposed development is suitable.

The scale and character of the proposed development is generally in keeping with the surrounding context. The surrounding area consists of low and medium-density residential uses with emerging opportunities for intensification. The proposed six-storey apartment building connected by a central common lobby corridor are compatible with the evolving physical character of the area.

The subject site is one of the larger underutilized residentially designated parcels in this area and therefore has the potential to accommodate future growth as intended by the Official Plan Intensification Area. The proposed six-storey building accommodates a total of 108 condominium dwelling units within a gross floor area of approximately

3,623.2 m<sup>2</sup> (39,000 ft<sup>2</sup>). The proposed residential infill development will contribute to the Province's goal of building 1.5 million homes by 2031.

The proposal features a maximum building height of six (6) storeys and a residential form consistent with high-density permissions. This represents an appropriate level of intensification given the Site's access via the Croft Street extension, proximity to surrounding road networks, and the provision of adequate parking and servicing.

While the proposed density is greater than existing low-density residential development in the immediate vicinity, the design efficiently accommodates parking below-grade, with 132 underground spaces, thereby reducing surface parking impacts. Only 26 at-grade parking spaces are proposed along the Croft Street frontage, allowing for enhanced landscaped areas and outdoor amenity space on the southeastern portion of the Site.

It is our professional planning opinion that the proposed development is consistent with the growth policies in the PPS and generally conforms to the general intent and purpose of the Northumberland and Port Hope Official Plans and Zoning framework, subject to approval of the requested Official Plan and Zoning By-law Amendments.

## 6.2 Complete Community

On a broad scale, the proposed development will support the creation of a complete community within the Municipality of Port Hope. The proposal contemplates the intensification of a vacant residential site through the development of a six-storey residential apartment building. The proposal will add 108 new residential units to Port Hope's housing stock, contributing to the range and type of housing available in the community. The proposed units will provide a range of suite sizes suitable for a variety of household types, ages, and needs.

The proposed development is located in an area that is served by the municipal road network and is supported by future extension of Croft Street. The Site is proximate to planned and existing pedestrian and cycling routes and is accessible to local transit services. The redevelopment of the subject site will encourage the use of active transportation and support future transit connectivity, contributing to a more balanced, multi-modal transportation system within Port Hope.

The proposal will contribute to a complete community by introducing new residential units, enhanced landscaped areas, outdoor amenity space, and public realm improvements along Croft Street. The redevelopment of the vacant, underutilized site will represent a net-positive contribution to the overall vitality and long-term growth objectives of the surrounding neighbourhood.

The proposal is sensitive to the existing and planned context, and provides appropriate transitions in height and scale to adjacent properties. The surrounding area is predominantly characterized

by low- to medium-density residential uses. The proposed six-storey building height represents a planned and appropriate scale of development given the site's designation and emerging development context. The buildings are sited to provide appropriate setbacks ensuring suitable transitions in massing and built form.

It is our professional planning opinion that the proposed development is consistent with the growth policies in the PPS and generally conforms to the Northumberland and Port Hope Official Plans and Zoning framework, subject to approval of the requested Official Plan and Zoning By-law Amendments.

## 7.0 CONCLUSION

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Based on the information outlined in this Planning Justification Report and the findings identified in the supporting technical reports and studies, it is our professional planning opinion that the proposed development, Official Plan and Zoning By-law Amendments represent an appropriate improvement of the subject site.

The proposal represents good planning, as it supports appropriate growth and development on a site that benefits from access to existing and future planned municipal infrastructure and proximity to nearby amenities.

We conclude the following:

- The proposed development, Official Plan Amendment, and Zoning By-law Amendment are consistent with the Provincial Planning Statement;
- The proposed development, Official Plan Amendment, and Zoning By-law Amendment conforms to the County of Northumberland Official Plan;
- The proposed development, Official Plan Amendment, and Zoning By-law Amendment generally conforms to the general intent and purpose conform to the Municipality of Port Hope Official Plan;
- The proposed development will not create adverse impacts on the surrounding area; and
- The proposed development can be appropriately serviced by existing and planned infrastructure.

It is our professional planning opinion that the proposed development is appropriate, desirable and achieves good planning.

Respectfully,



Hussnain Mohammad, B. URPI  
Urban Designer  
RFA Planning Consultant Inc.



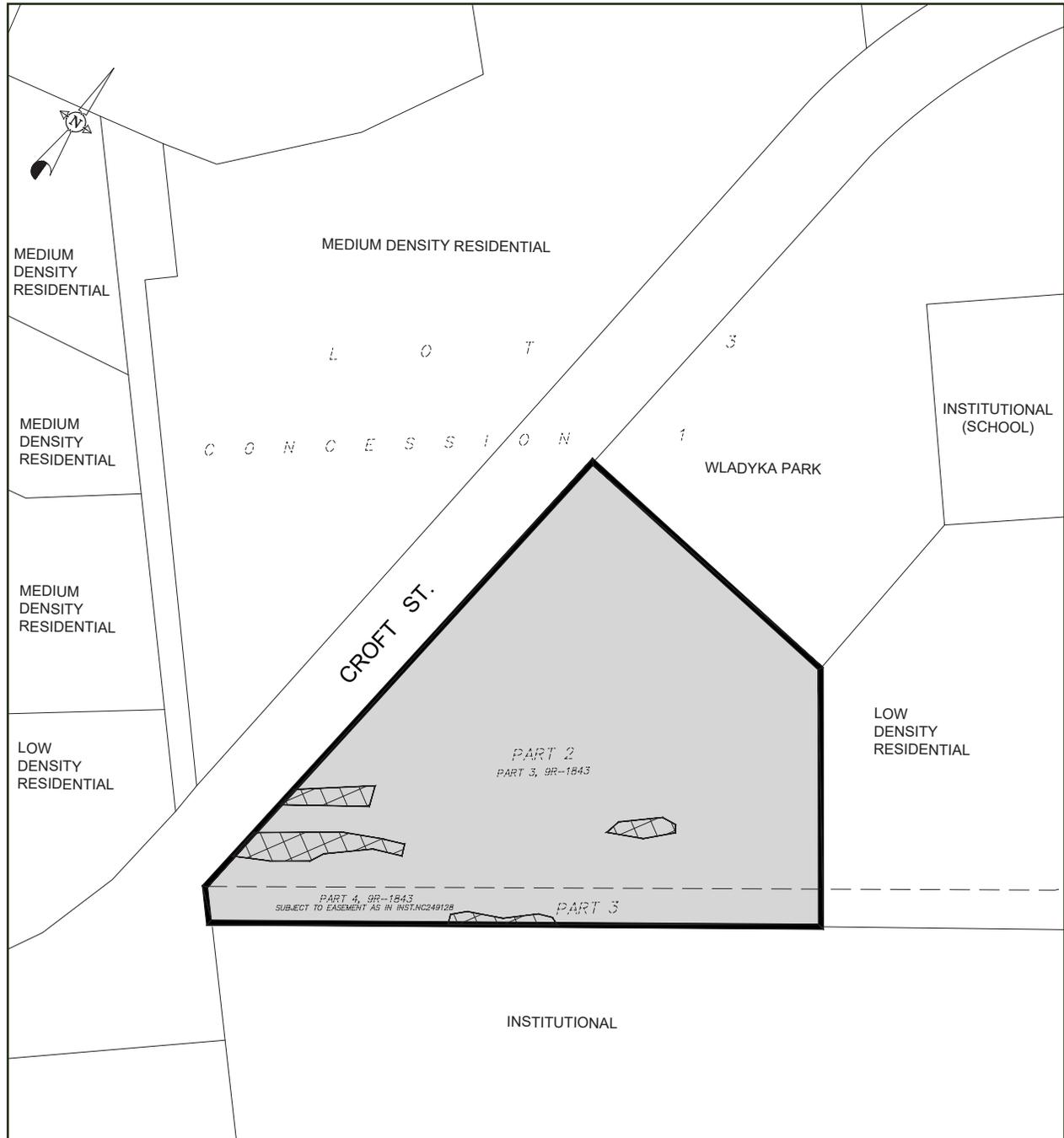
Shawn Legere, MCIP RPP  
President / Principal  
RFA Planning Consultant Inc.



# APPENDICES

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**Draft Official Plan Amendment**



**LEGEND:**

- LANDS TO BE DESIGNATED FROM MEDIUM DENSITY RESIDENTIAL TO "HIGH DENSITY RESIDENTIAL"
- LANDS DESIGNATED WITH A "PHYSICAL CONSTRAINT AREA" OVERLAY TO REMAIN

LOCATION  
 PART OF LOT 3,  
 CONCESSION 1,  
 GEOGRAPHIC TOWNSHIP OF HOPE,  
**MUNICIPALITY OF PORT HOPE**  
 COUNTY OF NORTHUMBERLAND

OFFICIAL PLAN AMENDMENT No. \_\_\_\_\_  
 TO OFFICIAL PLAN - SCHEDULE "C-1"  
 LAND USE - URBAN AREA  
**SCHEDULE '1'**

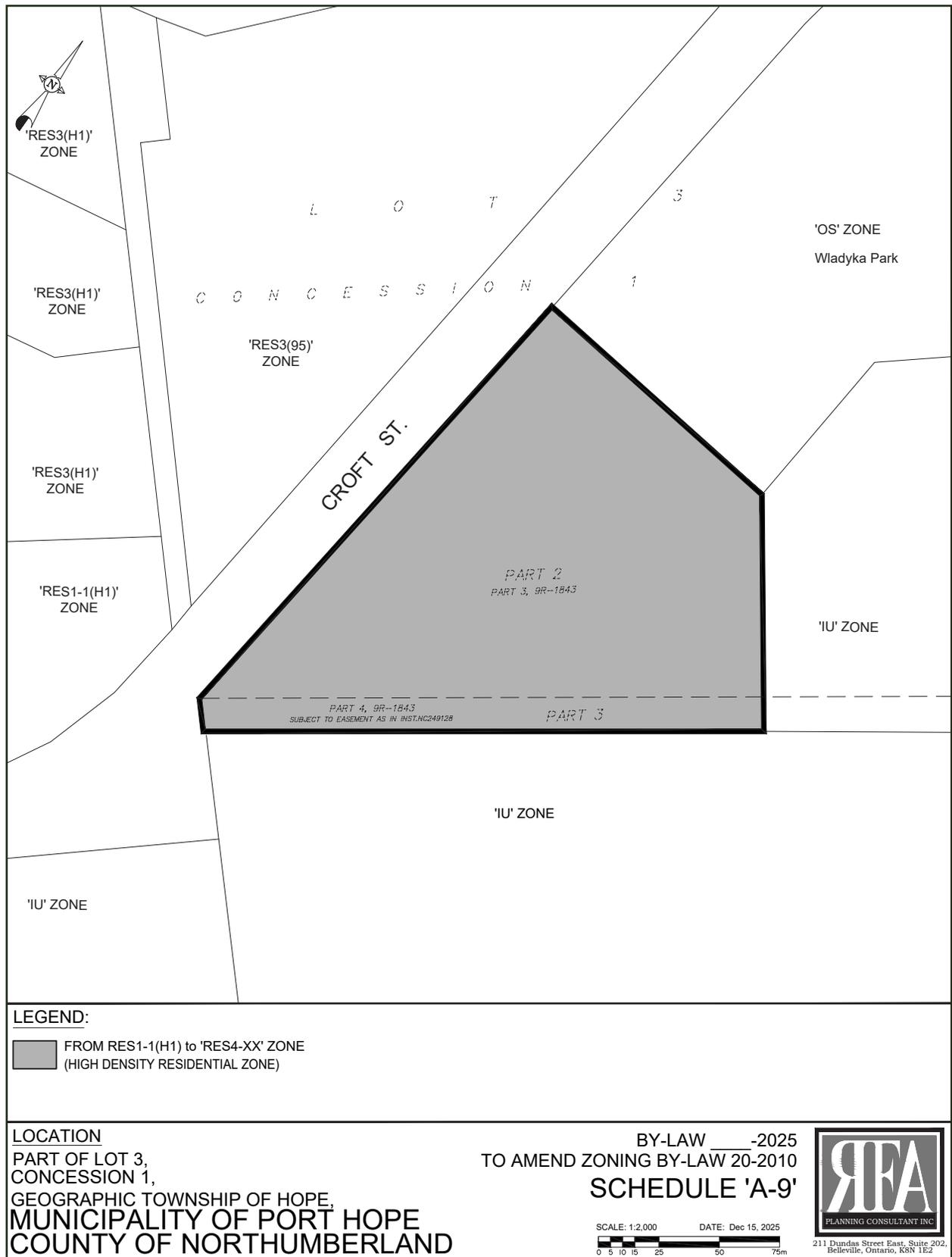
SCALE: 1:2,000 DATE: DEC 15 2025



211 Dundas Street East, Suite 202,  
 Belleville, Ontario, K8N 1E2



### Draft Zoning By-law Amendment



THE CORPORATION OF THE MUNICIPALITY OF PORT HOPE

BY-LAW NO. XX-2025

Being a By-law Under the Provisions of Section 34 of the Planning Act, R.S.O., 1990, c.P.13, as Amended, to Amended Zoning By-law 20/2010, as Amended of the Corporation of the Municipality of Port Hope, for lands described as Part of Lot 3, Concession 1, Registered Plan No. 39, in the Municipality of Port Hope, in the County of Northumberland.

WHEREAS Zoning By-law No. 20/2010, as amended by By-law 31/2023, as otherwise amended, was passed under authority of Section 34 of the Planning Act, R.S.O. 1990, c.P.13, as amended; and

WHEREAS this By-law conforms with the general intent of the Municipality of Port Hope Official Plan, as amended; and

WHEREAS the Council of The Corporation of the Municipality of Port Hope conducted a public meeting, in regard to the proposed zoning, as required by Section 34(12) of the Planning Act. R.S.O. 1990, c.P.13, as amended; and

WHEREAS the Council of the Corporation of the Municipality of Port Hope deems it advisable to amend Zoning By-law No. 20/2010, as amended by By-law 31/2023, as otherwise amended, with respect to the above described lands, and under the provisions of the Planning Act has the authority to do so;

NOW THEREFORE the Council of The Corporation of the Municipality of Port Hope ENACTS as follows:

1. THAT Schedule "A" - Sheet 9 (Zone Map) forming part of the Zoning By-law 20/2010, as amended by By-law 31/2023, as otherwise amended, is hereby amended by changing the zone classification of the subject lands from the current Residential One-Type One Holding Symbol 1 'RES1-1(H1)' zone to High Density Residential Exception XX Zone (RES4-XX);
2. THAT Zoning By-law No. 20/2010, as amended by By-law 31/2023, as otherwise amended, is hereby amended by adding the following to Part 12 - Exceptions:

Table 6: Site Specific Provisions for High Density Residential 'RES4' Zone		
Provision		Proposed
4.25.1.d)	Where there is an opaque wall or fence having a height of 1.5 metres or more, the width of the planting strip is permitted to be reduced to 1.8 metres in width.	2.1 metres
5.5.3.1.b)	The maximum driveway width shall be equal to the garage door width plus 1.5 metres or 6.1 metres, whichever is greater, provided a minimum of 40% of the area of the front exterior side yard in which the driveway is located is the site of soft landscaping.	8 metres
5.5.3.3	On lots that have a lot frontage of 22.0 metres or greater, a second driveway crossing the same lot line as the first driveway is permitted provided:	Three (3) driveways
5.7.2.c)	Not be permitted in the front yard or exterior side yard, and shall not be located in any required yard;	No, loading spaces are located in the front yard



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